

REPUBLIC OF THE PHILIPPINES
DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS
REGION VIII
TACLOBAN CITY DISTRICT ENGINEERING OFFICE
NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY

CY 2025 PROJECT
DETAILED ENGINEERING DESIGN PLAN FOR
REPAIR/ MAINTENANCE
OF
DAMAGED PAVED ROAD - K0907+209 - K0907+321.5
ALONG TIGBAO- STA.FE- SAN MIGUEL ROAD (S00033LT)

LENGTH: 75.37 LINEAR METER

SUBMITTED:

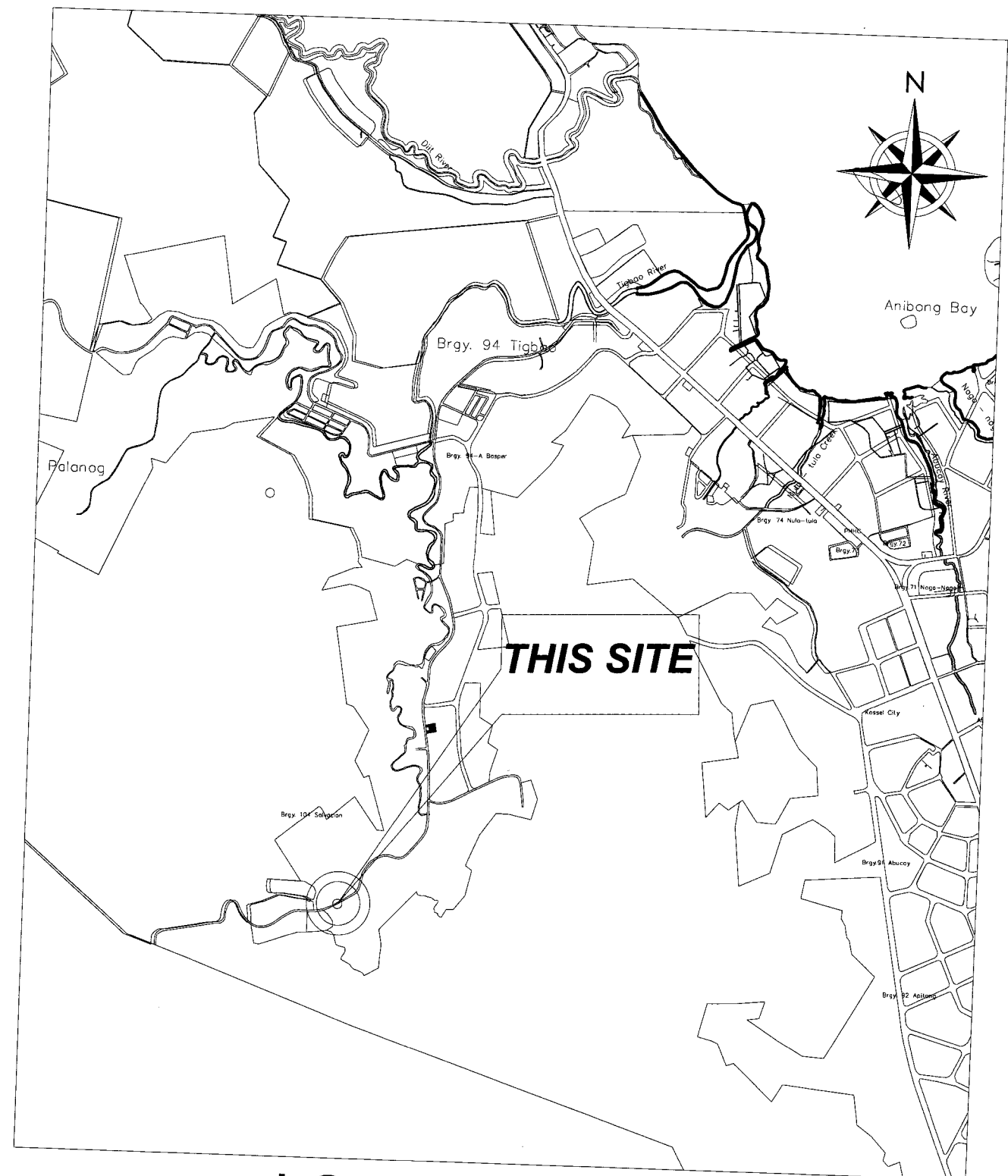
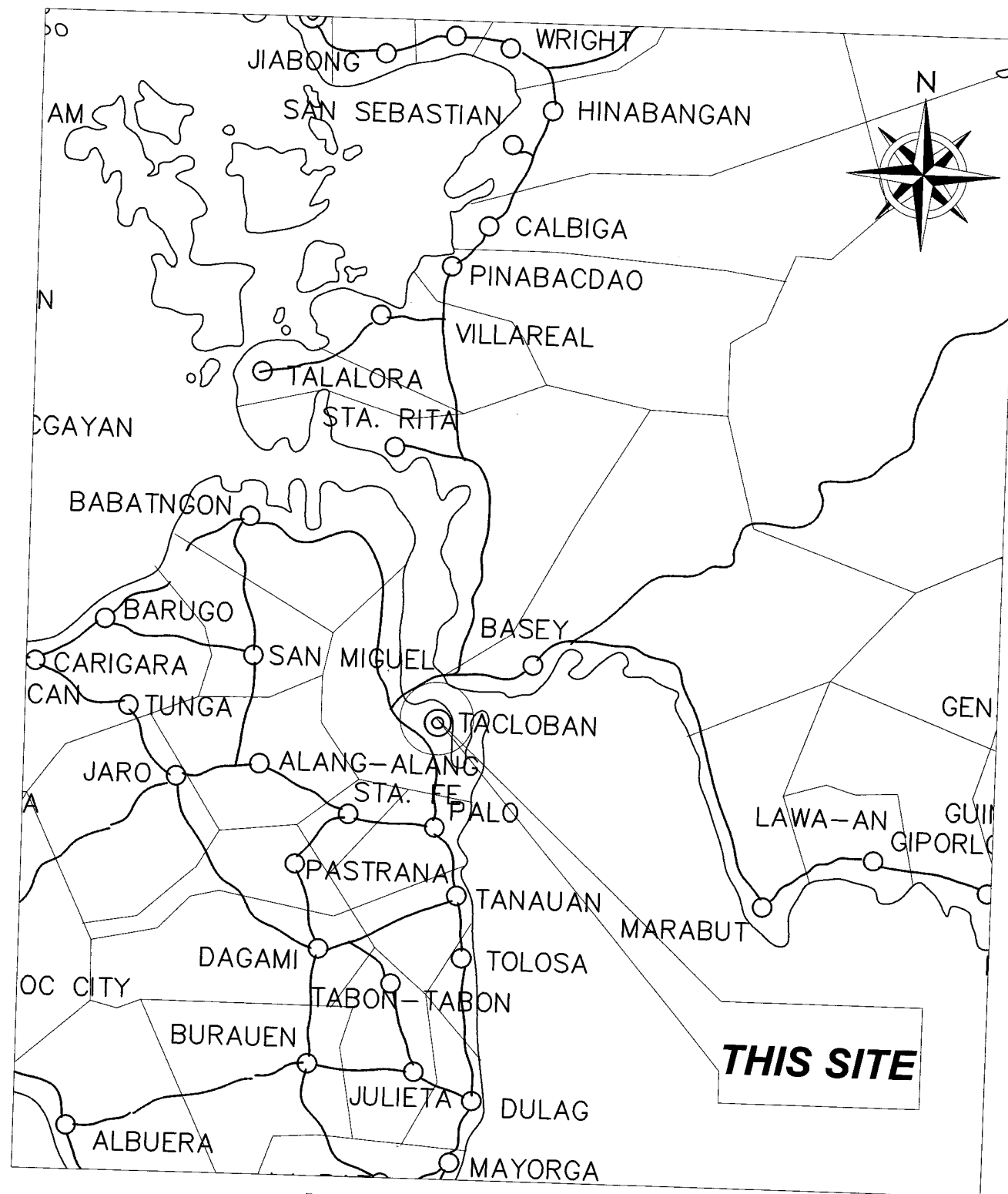
MARY GRACE L. GADDI
ENGINEER II, MAINTENANCE SECTION
DATE:

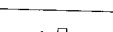






RECOMMENDED:

CHARLIE B. TAMPIL
OIC - ASSISTANT DISTRICT ENGINEER
DATE:

APPROVED:

RAYE MATE
OIC - DISTRICT ENGINEER
DATE: 09/18/2025



 <p>REPUBLIC OF THE PHILIPPINES DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS REGION VIII TACLOBAN CITY DISTRICT ENGINEERING OFFICE NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY</p>	PROJECT NAME AND LOCATION:	SHEET CONTENTS:	DRAFTED:	REVIEWED:	SUBMITTED:	RECOMMENDED:	APPROVED:	SET NO.	SHEET NO.
	REPAIR/ MAINTENANCE OF DAMAGED PAVED ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO- STA.FE- SAN MIGUEL ROAD (S00033LT)		VICINITY MAP LOCATION MAP	 JONAS F. POSTRETERO ENGINEER	 REY R. MECINO ENGINEER II	 MARY GRACE L. GADDI ENGINEER II, MAINTENANCE SECTION	 CHARLIE B. TAMPIL OIC - ASSISTANT DISTRICT ENGINEER	 RAYMUNDO OIC - DISTRICT ENGINEER	

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SUMMARY OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QTY.	REMARKS
B.5	Project Billboard/Signboard	ea.	4.00	
B.7(2)	Occupational Safety and Health Program	l.s.	1.00	
B.8(1)	Traffic Management	l.s.	1.00	
B.9	Mobilization / Demobilization	l.s.	1.00	
101(3)b5	Removal of Actual Structures/Obstruction, 0.28m thick, PCCP(Unreinforced)	sq.m.	120.60	
105(1)b	Subgrade Preparation (Existing Pavement)	sq.m.	120.60	
200(1)	Aggregate Subbase Course	cu.m.	24.12	
311(1)e3	Portland Cement Concrete Pavement (Unreinforced), 0.28m thick, 3 days	sq.m.	120.60	

GENERAL NOTES

I. DESIGN

- A.) THE REHABILITATION/ CONSTRUCTION PROJECT FOLLOWS THE EXISTING TRAVERSE AND GROUND ELEVATION

II. DESIGN STANDARDS

- A.) DPWH DESIGN GUIDELINES, CRITERIA AND STANDARDS (DGCS), VOLUME 4, 2015 EDITION
B.) AASHTO A POLICY ON GEOMETRIC DEIGN STANDARDS OF HIGHWAYS AND STREETS, 2011, 6TH EDITION
C.) AASHTO GUIDE ON PAVEMENT DESIGN, 1993 EDITION
D.) HIGHWAY SAFETY DESIGN STANDARDS: PART 1 - ROAD SAFETY DESIGN, AND PART 2 - ROAD SIGNS AND PAVEMENT MARKINGS, 2012 EDITION

III. STANDARD SPECIFICATIONS

- A.) ALL WORKS SHALL COMPLY WITH THE DPWH STANDARD SPECIFICATIONS FOR HIGHWAYS, BRIDGES AND AIRPORTS, 2013 EDITION VOLUME 2.
B.) SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS PERTAINING TO THE PROJECT.

IV. DIMENSIONS

- A.) UNLESS OTHERWISE SPECIFIED, ALL DIMENSIONS WHICH INCLUDES STATIONING, DISTANCE BETWEEN CONTROL POINTS AND AS SHOWN IN THE PLAN ARE IN METER AND THE UNIT OF MEASURE AS SHOWN IN DETAILS OF STRUCTURE ARE IN MILLIMETERS.

V. TOPOGRAPHIC SURVEY

- A.) SHALL BE DONE AS PER TERMS OF REFERENCE

V.1.) STATIONING

- A.) THE ROAD STATIONING AND ELEMENTS OF ELEMENTS OF HORIZONTAL AND VERTICAL CURVES SHOWN ON THE PLAN AND PROFILE SHEETS ARE RECKONED FROM THE ROADWAY CENTERLINE.
B.) STATIONING OF THE BRIDGES, RCPC, RCBC, AND OTHER STRUCTURES ARE RECKONED FROM THE STATIONING OF THE ROADWAY CENTERLINE SHOWN ON THE PLAN.

VI. ELEVATIONS AND GRADES

- A.) FINISHED GRADE ELEVATION SHOWN ON PLAN AND PROFILE SHEETS REFERS TO THE FINISHED PAVEMENT LEVEL SHOWN ON THE TYPICAL ROADWAY SECTION.
B.) GROUND GRADE SHOWN ON THE PLAN AND PROFILE SHEET REFERS TO THE ELEVATION OF THE ORIGINAL GROUND ALONG THE CENTERLINE OF THE PROJECT ROAD.
C.) FINISHED GRADE FOR THIS PROJECT ARE SUBJECT TO CHANGE TO SUIT EXISTING FIELD CONDITION HOWEVER THAT IT IS MORE ADVANTAGEOUS AND MORE ECONOMICAL ON THE PART OF THE GOVERNMENT AND THE DESIGN STANDARD FOR HIGHWAYS PER REQUIREMENT OF AASHTO ARE PROPERLY FOLLOWED.
D.) WIDENING IN CURVES IS SUBJECTED TO ADJUSTMENT TO SUIT EXISTING FIELD CONDITION AND SHALL BE BACKFIELD WITH APPROVED MATERIALS.
E.) PROPER ROAD CONNECTION AT THE BEGINNING AND END OF THE PROJECT SHALL BE PROVIDED TO ENSURE SMOOTH RIDING SURFACE.

VII. REMOVAL OF EXISTING STRUCTURES AND OBSTRUCTIONS

- A.) ALL WORKS SHALL COMPLY WITH CLAUSE 39 REQUIREMENTS AND CONDITIONS OF CONTRACTS VOLUME-1 OF THE STANDARD SPECIFICATIONS FOR PUBLIC WORKS AND HIGHWAYS 1995.
B.) PORTIONS OF EXISTING UTILITIES SUCH AS WATER MAINS, IRRIGATION CHANNELS, TELEPHONE POSTS AND TRUNK LINE, ETC. THAT MAY CAUSE OBSTRUCTION TO THE CONSTRUCTIONS OWNER CONCERNED, EXTREME PRECAUTION SHALL BE EXERCISED BY THE CONTRACTOR NOT TO DAMAGE ANY SECTION OF THE EXISTING PUBLIC UTILITIES DURING CONSTRUCTION, ANY REPAIR OF DAMAGE HEREOF SHALL BE ON THE ACCOUNT OF THE CONTRACTOR, ANY REMOVAL OF THE MISCELLANEOUS STRUCTURES THAT MAY BE REQUIRED SHALL BE SUBSIDIARY WORK PERTAINING TO OTHER CONTRACT ITEM. NO DIRECT PAYMENT SHALL BE MADE FOR THIS EXCEPT FOR SPECIFIC ITEMS EXPLICITLY IDENTIFIED FOR PAYMENT IN THE BID SCHEDULE.

VIII. ROAD CONNECTIONS AND PRIVATE ENTRANCES

- A.) APPROACHES AND PRIVATE ENTRANCES SHALL BE CONSIDERED BY THE CONTRACTOR AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER IN SUCH A MANNER TO ENSURE SMOOTH CONNECTIONS AND RIDING QUALITY.
B.) NO OPENING FOR DRIVEWAYS OR PRIVATE ENTRANCES SHALL BE ALLOWED EXCEPT WITH THE PRIOR APPROVAL FROM THE PROPER AUTHORITIES.

IX. THE IMPLEMENTING OFFICE SHALL IDENTIFY THE LOCATIONS OF AND PROVIDE ACCESSIBILITY FACILITIES FOR PERSONS WITH DISABILITY IN ACCORDANCE WITH D.O. 37 SERIES OF 2009

DESIGN SPECIFICATIONS

1.) PAVEMENT DESIGN CRITERIA

A.) PAVEMENT DESIGN PARAMETER:

DESIGN REQUIREMENTS	
A. PERFORMANCE PERIOD FOR PCCP	20 YEARS (FOR PCCP)
B. DESIGN TRAFFIC: ESAL	3.81E+06
C. DESIGN RELIABILITY: R	0.85
D. STANDARD DEVIATION: S _o	0.35
E. DESIGN SERVICEABILITY LOSS: APSI	2.00
F. PCCP MODULUS OF RUPTURE: S _c	635.55
G. PCCP MODULUS OF ELASTICITY: E _c	3.37E+06
H. SUBGRADE DESIGN CBR	25.06
I. EFFECTIVE ROADBED RESILIENT MODULUS: MR	37,585.79
J. SUBBASE ELASTIC MODULUS: ESB	15,000.00
K. SUBBASE THICKNESS	200.00 MM
L. EFFECTIVE MODULUS AT SUBGRADE REACTION: K (pci)	900.00
M. DRAINAGE COEFFICIENT: J	1.00
N. LOAD TRANSFER COEFFICIENT: J	4.10
O. LOSS OF SUPPORT: L _s	1.00
P. K (corrected):	230.00

COORDINATE REFERENCE SYSTEM	PRS92 / PHILIPPINE ZONE V
PROJECTION:	TRANSVERSE MERCATOR (TM) IN ZONE OF 2° NET WIDTH
DATUM:	PHILIPPINE REFERENCE SYSTEM 1992/WGS84
EPSG CODE:	3125

REFERENCE BENCHMARK DETAILS

- 1.) THE POSITION OF PROJECT CONTROL POINTS SHALL BE DEFINED AND MARKED ON THE GROUND BY MONUMENT IF PERMANENT NATURE

2.) CRITERIA FOR LOCATION MONUMENTS

- ACCESSIBILITY
- GROUND STABILITY
- SECURITY FROM POSSIBLE ACTS OF DISTURBANCE

3.) INTERVAL OF MONUMENTS

- PRIMARY GPS CONTROLS (GPS): 3KM INTERVAL
- PRIMARY PROJECT CONTROL (BM): 500M INTERVAL
- INTERMEDIATE CONTROL (IBM): EVERY 250M INTERVAL IN BETWEEN IBM'S

EQUIPMENT USED: TOPCON / RTK GR5 - BASE SER.NO.1118-23692, ROVER SER.NO.1118-23953, CONTROLLER NO. 224852



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PROJECT NAME AND LOCATION:
REPAIR/ MAINTENANCE OF DAMAGED PAVED
ROAD - K0907+209 - K0907+321.5 ALONG
TIGBAO- STA.FE- SAN MIGUEL ROAD (S00033LT)

SHEET CONTENTS:
GENERAL NOTES

DRAFTED:

JONAS F. POSTERO
ENGINEER I

PREPARED:

ELIZAR JEROME BACLEA-AN
ENGINEER II

REVIEWED:

REY R. MECINO
ENGINEER II

SUBMITTED:

MARY GRACE L. GADDI
ENGINEER IV, MAINTENANCE SECTION

RECOMMENDED:

CHARLIE B. JAMPIL
OIC - ASSISTANT DISTRICT ENGINEER

APPROVED:

RAYMUNTE
OIC - DISTRICT ENGINEER

SET NO.

4

SHEET NO.

17

CONSTRUCTION REQUIREMENT

1. ALL CONSTRUCTION SHALL CONFORM TO:

- CONDITIONS OF CONTRACT
- THE SPECIAL PROVISIONS
- THE SPECIFICATIONS OF ITEMS OF WORK FOR THIS PROJECT SHALL BE THE DPWH STANDARD SPECIFICATIONS FOR PUBLIC WORKS & HIGHWAYS 2013 EDITION, PRESENTED IN THE TENDER DOCUMENTS OF THE PROJECT.

2. SETTING OUT

- THE SETTING OUT AND ELEVATION OF THE DIFFERENT COMPONENTS OF THE STRUCTURE SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE START OF ANY CONSTRUCTION WORK.

3. EXCAVATION

- EXCAVATION FOR STRUCTURES SHALL BE NEAT LINES AS SHOWN IN THE PLANS AND THE SOIL UNDERNEATH STRUCTURE FOUNDATION SHALL NOT BE DISTURBED.

4. REINFORCED CONCRETE

A. CONCRETE MIX AND PLACING

- DESIGN OF CONCRETE MIX SHALL MEET THE DESIGN CONCRETE STRENGTH GIVEN UNDER ITEM 1 OF MATERIALS
- CONCRETE SHALL BE DEPOSITED, VIBRATED AND CURED IN ACCORDANCE WITH THE SPECIFICATIONS.
- FOR CONCRETE DEPOSITED AGAINST THE GROUND, LEAN CONCRETE WITH A MINIMUM THICKNESS OF 50 MM SHALL BE LAID FIRST BEFORE INSTALLING THE REINFORCING. THIS LEAN CONCRETE SHALL NOT BE CONSIDERED IN MEASURING THE STRUCTURAL DEPTH OF CONCRETE SECTION.

B. CONSTRUCTION JOINT

- THE POSITION AND FORM OF ANY CONSTRUCTION JOINT SHALL AS SHOWN ON DRAWINGS OR AS AGREED WITH THE ENGINEERS.

C. FALSEWORK

- ALL FALSEWORK SHALL BE DESIGN BY THE CONTRACTOR SUBJECT TO THE APPROVAL BY THE ENGINEER.

5. EMBANKMENT

- PRIOR TO CONSTRUCTION OF EMBANKMENT, ALL NECESSARY CLEARING & GRUBBING IN THE AREA SHALL BE PERFORMED IN CONFORMITY WITH ITEM 100.
- ALL UNSUITABLE MATERIALS, OTHER THAN DELIVERED SUITABLE MATERIALS, SHALL BE DISPOSED OF IN THE MANNER SPECIFIED IN THIS ITEM OR AS DIRECTED BY THE ENGINEER.
- CONSTRUCTION OF ROADWAY EMBANKMENTS INCLUDES PREPARATION OF THE AREAS UPON WHICH SELECTED MATERIALS ARE TO BE PLACED, PLACING AND COMPACTING EMBANKMENT MATERIALS IN HOLES, PITS AND OTHER DEPRESSION WITHIN THE ROADWAY AREA.

6. ALIGNMENT AND GRADE

- FINISHED GRADE FOR THIS PROJECT ARE SUBJECT TO CHANGE TO SUIT EXISTING FIELD CONDITION HOWEVER THAT IT IS MORE ADVANTAGEOUS AND MORE ECONOMICAL ON THE PART OF THE GOVERNMENT AND THE DESIGN STANDARD FOR HIGHWAYS PER REQUIREMENT OF AASHTO AREA PROPERLY FOLLOWED.
- WIDENING IN CURVES IS SUBJECTED TO ADJUSTMENT TO SUIT EXISTING FIELD CONDITION AND SHALL BE BACKFIELD WITH APPROVED MATERIALS.
- PROPER ROAD CONNECTION AT THE BEGINNING AND END OF THE PROJECT SHALL BE PROVIDED TO ENSURE SMOOTH RIDING SURFACE.

7. SUB-GRADE, SUB-BASE AND BASE

- UNSUITABLE SUB-GRADE MATERIALS SHALL BE EXCAVATED BELOW THE GROUND SURFACE TO THE REQUIRED WIDTH AND DEPTH, THE AREA EXCAVATED SHALL BE BACKFILLED WITH THE APPROVED MATERIALS.
- NO EMBANKMENT MATERIALS SHALL BE PLACED UNTIL THE FOUNDATION IS STABLE.

8. CONCRETE AND CONCRETE PAVEMENT

- TRAFFIC SHALL BE REQUIRED TO REDUCE SPEED WHEN PASSING THE VICINITY OF THE NEWLY LAID CONCRETE PAVEMENT UNTIL SUCH TIME THAT IT HAS OBTAINED THE FOURTEEN (14) DAYS REQUIRED CURING PERIOD.
- NO ADMIXTURES OR ADDITIVES WILL BE ALLOWED FOR ALL CONCRETE WORKS WITHOUT PRIOR APPROVAL FROM THE SECRETARY OF DPWH OR HIS DULY APPOINTED REPRESENTATIVES.
- WHEN CONCRETING OF PAVEMENT PROGRESSES TRAFFIC SHALL BE MADE TO PASS OUTSIDE THE EMBANKMENT PRISM IN ORDER TO MINIMIZE THE EFFECT OF VIBRATION TO THE SETTING CONCRETE.
- THE EXISTING CONCRETE CURB AND GUTTER THAT INTERFERES IN THE CONSTRUCTION SHALL BE REMOVED.
- THE CONTRACTOR SHALL SUBMIT A SUPERSTRUCTURE PLACING SEQUENCE FOR THE ENGINEER'S APPROVAL.

F. CONCRETE STRENGTH BY CLASS

CLASS	28 DAYS CYLINDER STRENGTH		MAX. SIZE OF COARSE AGGREGATES mm (in)
	MPa	PSi	
A	20.7	3,000	38 (1-1/2)
B	16.5	2,400	50 (2)
C	20.7	3,000	12.50
P	37.7	5,000	19 (3/4)
LEAN	9.9	1,400	

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9. DRAINAGE STRUCTURES

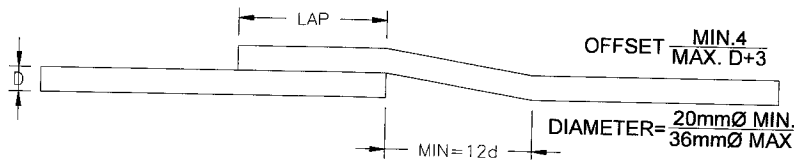
- EXACT LOCATIONS, SLOPES, OUTFALL, AND INVERT ELEVATIONS OF DRAINAGE STRUCTURES SHALL BE CHECKED IN THE FIELD, ADJUSTMENT MAY BE MADE TO SUIT ACTUAL FIELD CONDITIONS WITH APPROVAL OF THE ENGINEER.
- EXISTING DRAINAGE STRUCTURES OR PARTS THEREOF REMOVED BY THE CONTRACTOR WHICH ARE STILL SERVICEABLE SHALL BE DEPOSITED AT A PLACE DESIGNATED BY THE ENGINEER WITHIN THE PROJECT SITE WITHOUT ANY COMPENSATION. EXTREME PRECAUTIONS SHALL BE EXERCISED BY THE CONTRACTOR SO AS NOT TO DAMAGE THESE MATERIALS DURING THE REMOVAL AND HANDLING.
- PORTIONS OF EXISTING UTILITIES SUCH AS WATER MAINS, IRRIGATION CHANNELS, TELEPHONE POSTS AND TRUNK LINE, ETC. THAT MAY CAUSE OBSTRUCTION TO THE CONSTRUCTIONS OWNER CONCERNED, EXTREME PRECAUTION SHALL BE EXERCISED BY THE CONTRACTOR NOT TO DAMAGE ANY SECTION OF THE EXISTING PUBLIC UTILITIES DURING CONSTRUCTION, ANY REPAIR OF DAMAGE HEREOF SHALL BE ON THE ACCOUNT OF THE CONTRACTOR. ANY REMOVAL OF THE MISCELLANEOUS STRUCTURES THAT MAY BE REQUIRED SHALL BE MADE FOR THIS EXCEPT FOR SPECIFIC ITEMS EXPLICITLY IDENTIFIED FOR PAYMENT IN THE BID SCHEDULE.

10. MISCELLANEOUS STRUCTURES

- LOCATION AND LENGTH OF SLOPE PROTECTIONS, GUARDRAILS, STONE MASONRY RETAINING WALLS AND OTHER STRUCTURES MAY BE ADJUSTED BY THE CONTRACTOR TO SUIT ACTUAL FIELD CONDITIONS WITH THE APPROVAL OF THE ENGINEER.

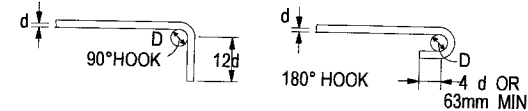
11. REINFORCING STEEL

- THE CONTRACTOR/BIDDER SHALL SUBMIT SHOP DRAWINGS TO THE ENGINEER FOR APPROVAL PRIOR TO FABRICATION. SHOP DRAWING SHOULD SHOW DETAILS FOR FABRICATION AND FOR PLACING REINFORCING STEEL ONLY THOSE NECESSARY FOR THE PROPER LOCATION OF THE STEEL ARE REQUIRED ON THE DRAWINGS. BENDING DETAILS MAY BE SHOWN ON A SEPARATE SHEET.
- NO MORE THAN ONE BAR IN THERE SHALL BE SPLICED AT THE SAME SECTION UNLESS OTHERWISE SHOWN.
- SPLICING SHALL BE KEPT TO A MINIMUM AND SHOULD BE STAGGERED AND LAPPED NOT LESS THAN 40 BAR DIAMETER UNLESS OTHERWISE SHOWN ON DRAWING. WHERE THE CLEAR DISTANCE BETWEEN LAPPED BARS DO NOT MEET THE REQUIREMENTS IN ITEM 12.4 THE CONTRACTOR SHALL USED CRANKED SPLICES AS DETAILED BELOW.



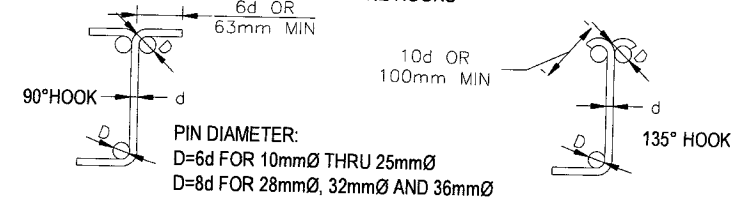
C. BAR SHALL BE BENT COLD. NO BAR PARTIALLY EMBEDDED IN CONCRETE SHALL BE FIELD BENT, UNLESS SHOWN ON THE DRAWN SPECIALLY PERMITTED BY THE ENGINEER. HOOKS SHALL BE FABRICATED AS SHOWN BELOW UNLESS OTHERWISE INDICATED ON THE DRAWING. BAR SHALL BE BENT AROUND A PIN WITH MINIMUM DIAMETER D.

DIMENSIONS OF 90 - DEGREE AND 180 - DEGREE HOOKS



PIN DIAMETER: D=6d FOR Ø10 THRU Ø25
D=8d FOR Ø28, Ø32 AND Ø36

DIMENSIONS FOR STIRRUPS AND TIE HOOKS



- UNLESS OTHERWISE SHOWN IN THE DRAWINGS, THE CLEAR DISTANCE BETWEEN PARALLEL BARS IN A LAYER SHALL NOT BE LESS THAN 1.5 TIMES THE NOMINAL DIAMETER OF THE BAR NOR LESS THAN 1.5 TIMES THE MAXIMUM SIZE OF COARSE AGGREGATE. THE CLEAR DISTANCE BETWEEN LAYERS SHALL NOT BE LESS THAN 25mm NOR ONE BAR DIAMETER. THE BARS IN THE UPPER LAYER SHALL BE PLACED DIRECTLY ABOVE THOSE IN THE BOTTOM LAYER.
- EXCEPT OTHERWISE SHOWN ON THE DRAWINGS, MINIMUM CONCRETE COVER SHALL BE PROVIDED FOR REINFORCEMENT AS FOLLOWS.

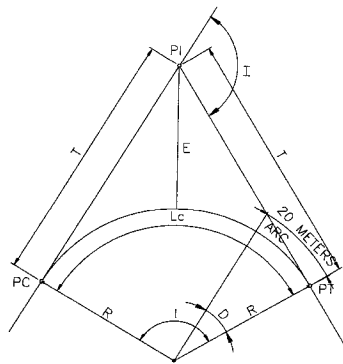
	MINIMUM CLEAR COVER mm (in)
CONCRETE IN A MARINE ENVIRONMENT OF CONCRETE CAST AGAINST AND PERMANENTLY EXPOSED TO EARTH CONCRETE EXPOSED TO EARTH OR WEATHER	75 (3)
PRIMARY REINFORCEMENT	50 (2)
STIRRUPS, TIES & SPIRAL	40 (1-1/2)
CONCRETE NOT EXPOSED TO WEATHER OR IN CONTACT WITH GROUND	40 (1-1/2)
PRIMARY REINFORCEMENT	40 (1-1/2)
STIRRUPS, TIES & SPIRAL	25 (1)
CONCRETE DECK SLABS	25 (1)
TOP REINFORCEMENT	50 (2)
BOTTOM REINFORCEMENT	25 (1)

NOTE:

THIS PLAN SHALL ONLY BE USED AS A GUIDE. SPECIFICALLY IN THE PRE-CONSTRUCTION STAGE. THE ACTUAL IMPLEMENTATION FOR THE PROJECT, ON THE HAND, WILL BE BASED ON THE "AS-STAKED" WHICH WILL BE DONE JOINTLY BY THE CONTRACTOR, THE IMPLEMENTING OFFICE, AND THE PLANNING AND DESIGN DIVISION, FOR THE ADDITIONAL REVIEW AND APPROVAL OF THE REGIONAL DIRECTOR.

REVISE THE TEMPLATE AND/OR STAKE OUT THE LOCATION OF LINED CANAL AND OTHER STRUCTURES AS PER TYPICAL ROADWAY SECTION BEFORE COMMENCING CONSTRUCTION. ADDITIONALLY, ANY CHANGES IN THE QUANTITY OF WORKS ITEM INVOLVED AS A RESULT OF REVISION MUST BE COMPUTED AND RE-CONSIDERED IN THE "AS-STAKED PLAN".

<p>REPUBLIC OF THE PHILIPPINES DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS REGION VIII TACLOBAN CITY DISTRICT ENGINEERING OFFICE NEW BUS TERMINAL, BRGY. 01, ABUCAY, TACLOBAN CITY</p>	PROJECT NAME AND LOCATION: REPAIR/ MAINTENANCE OF DAMAGED PAVED ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO- STA.FE- SAN MIGUEL ROAD (S00033LT)	SHEET CONTENTS: CONSTRUCTION REQUIREMENT	DRAFTED: JONAS F. POSTINERO ENGINEER I	REVIEWED: REY R. JACINO ENGINEER II	SUBMITTED: MARY GRACE AGADDI ENGINEER II, MAINTENANCE SECTION	RECOMMENDED: CHARLIE B. ZAMPIL OIC - ASSISTANT DISTRICT ENGINEER	APPROVED: RAY P. WHITE OIC - DISTRICT ENGINEER DATE: 07/19/2025	SET NO. 1	SHEET NO. 5 17
	<p>ELIZAR JEROME MACLEA-AN ENGINEER</p>								



FORMULA :

$$T = R \left(\tan \frac{I}{2} \right)$$

$$L_c = \frac{\pi}{180} IR$$

$$D = \frac{1145.916}{R}$$

$$E = T \left(\tan \frac{I}{4} \right)$$

LEGEND :

PI - POINT OF HORIZONTAL INTERSECTION
 I - EXTERNAL ANGLE
 T - LENGTH OF TANGENT
 R - HORIZONTAL RADIUS
 Lc - LENGTH OF CIRCULAR CURVE
 D - DEGREE OF CURVE (ARC DEFINITION)
 PC - POINT OF CURVATURE
 PT - POINT OF TANGENCY
 E - EXTERNAL DISTANCE

LEGEND :

PVI - POINT OF VERTICAL INTERSECTION
 PVC - POINT OF VERTICAL CURVATURE
 PVT - POINT OF VERTICAL TANGENCY
 LVC, LVC₁, LVC₂ - LENGTH OF VERTICAL CURVES
 g - GRADE IN PERCENT
 MO - MIDDLE ORDINATE
 X, X₁, X₂ - DISTANCE FROM PVC OR PVT TO ANY POINT ON CURVE
 Y, Y₁, Y₂ - VERTICAL OFFSET
 A - ALGEBRAIC DIFFERENCE OF GRADES IN %

NOTE :

NO VERTICAL CURVE IS REQUIRED WHEN THE ALGEBRAIC DIFFERENCE IN GRADE IS LESS THAN 0.5%

IN ANY SYMMETRICAL VERTICAL PARABOLIC CURVE

$$MO = \frac{ALVC}{800}$$

$$Y = \frac{X^2(MO)}{(LVC/2)^2}$$

FORMULA :

$$LVC = KA$$

LEGEND :

A - ALGEBRAIC DIFFERENCE OF GRADIENTS (%)
 K - RATE OF VERTICAL CURVATURE (m)

IN ANY UNSYMMETRICAL VERTICAL PARABOLIC CURVE

$$MO = \frac{(LVC_1)(LVC_2)(A)}{200LVC}$$

$$Y_1 = \frac{MO(X_1)^2}{(LVC_1)^2}$$

$$Y_2 = \frac{MO(X_2)^2}{(LVC_2)^2}$$

FORMULA :

$$LVC = KA$$

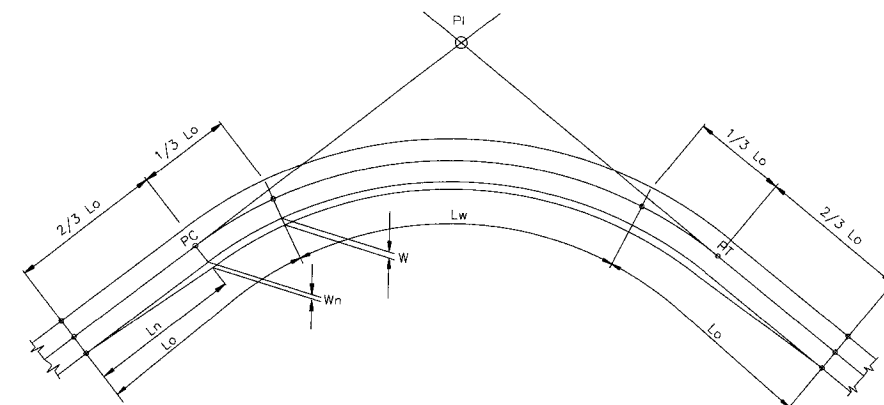
LEGEND :

A - ALGEBRAIC DIFFERENCE OF GRADIENTS (%)
 K - RATE OF VERTICAL CURVATURE (m)

DESIGN SPEED (km/h)	30 KPH	40 KPH	50 KPH	60 KPH	70 KPH
MIN. K FOR CREST VERTICAL CURVE	3	5	10	17	25
MIN. K FOR SAG VERTICAL CURVE	4	8.5	11	16	18.5

WIDENING OF PAVEMENT ON CURVE

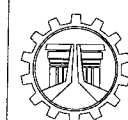
WIDENING (m)	RANGE	V=30 (km/h)	V=40 (km/h)	V=50 (km/h)	V=60 (km/h)
		RADIUS ABOVE 90	RADIUS ABOVE 105	RADIUS ABOVE 123	RADIUS ABOVE 143
0.6	0.550 ~ 0.699	89 ~ 73	104 ~ 84	122 ~ 97	142 ~ 112
0.8	0.700 ~ 0.899	72 ~ 58	83 ~ 66	96 ~ 76	111 ~ 86
1.0	0.900 ~ 1.099	57 ~ 48	65 ~ 54	75 ~ 61	85 ~ 69
1.2	1.100 ~ 1.299	47 ~ 41	53 ~ 46	60 ~ 51	68 ~ 58
1.5	1.300 ~	BELOW 41	BELOW 46	BELOW 51	BELOW 58



$$W_n = \frac{W L_n}{L_c}$$

LEGEND :

W = WIDENING WIDTH
 Lc = LENGTH OF SUPERELEVATION RUN-OFF
 Lw = LENGTH OF FULL WIDENING WIDTH
 Ln = LENGTH FROM THE BEGINNING OF RUN-OFF WITH A WIDENING OF Wn
 Wn = WIDENING OF LENGTH Ln



REPUBLIC OF THE PHILIPPINES
 DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS
 REGION VIII
TACLOBAN CITY DISTRICT ENGINEERING OFFICE
 NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY

PROJECT NAME AND LOCATION:
REPAIR/ MAINTENANCE OF DAMAGED PAVED ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO- STA.FE- SAN MIGUEL ROAD (S00033LT)

SHEET CONTENTS:
STANDARD GEOMETRIC DESIGN

DRAFTED:
JONAS F. FOSTERERO
 ENGINEER
 PREPARED:
ELIZAR JEROME MACLEA-AN
 ENGINEER II

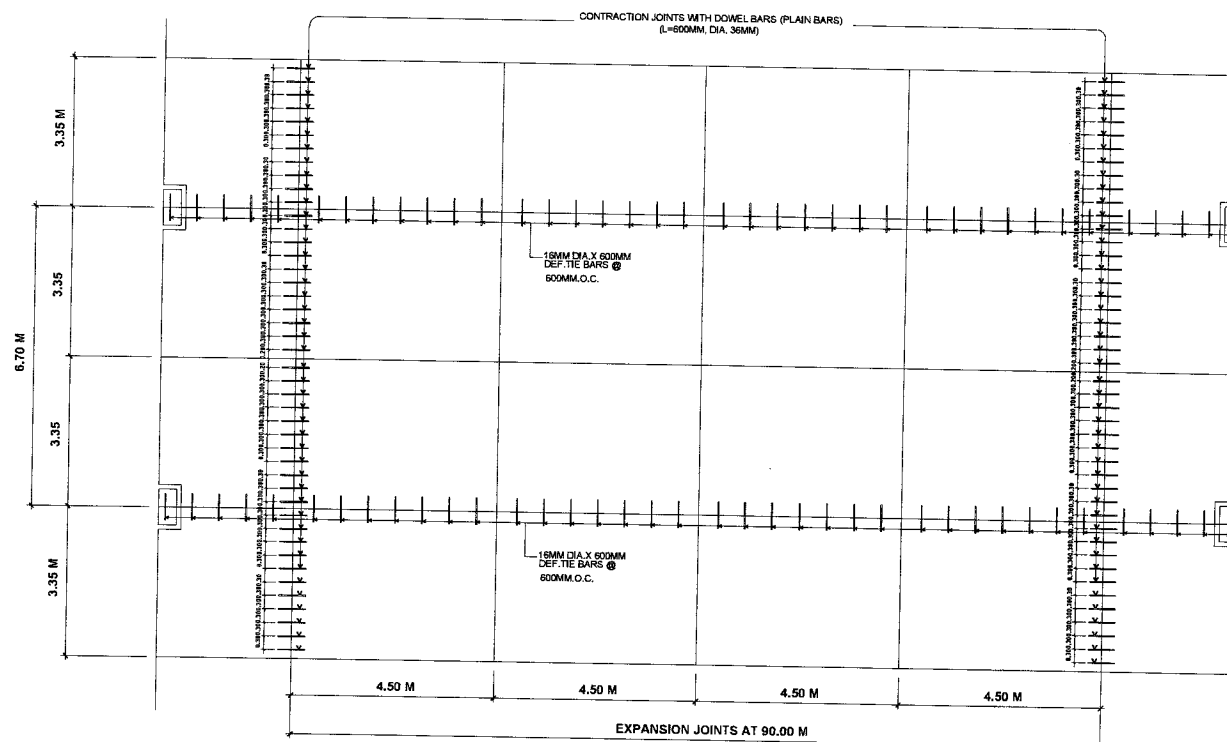
REVIEWED:
REY R. JECINO
 ENGINEER II
 DATE:

SUBMITTED:
MARY GRACE L. GADDI
 ENGINEER II, MAINTENANCE SECTION
 DATE:

RECOMMENDED:
CHARLIE P. TAMPIL
 OIC - ASSISTANT DISTRICT ENGINEER
 DATE:

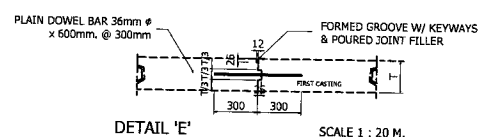
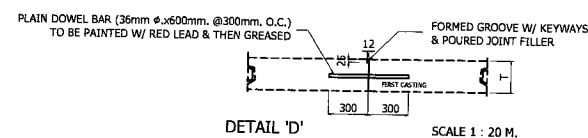
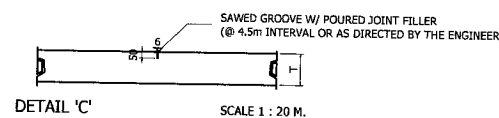
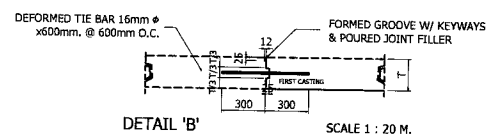
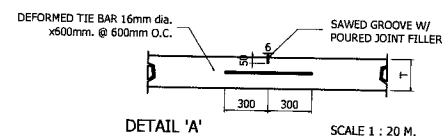
APPROVED:
RAY F. DATE
 OIC - DISTRICT ENGINEER
 DATE: 07/15/2025

SET NO. SHEET NO.
 6
 17



PAVEMENT LAYOUT PLAN

TYPICAL PLAN OF A FOUR LANE PAVEMENT
SCALE: NTS



KEYED TRANSVERSE CONSTRUCTION OR CONTRACT JOINT
(TO BE PLACED ONLY IN MIDDLE THIRD OF NORMAL JOINT INTERVAL)

NOTE:

TRANSVERSE CONSTRUCTION (CONTACT) JOINT WERE PROVIDED AT THE OF ANY RUN WHERE LAYING OF CONCRETE WERE STOPPED FOR THIRTY (30) MINUTES LONGER,

TRANVERSE CONSTRUCTION JOINTS WHICH OCCUR AT LOCATION OF WEAKENED PLANE, BUT JOINTS WITH DOWELS ARE USED, JOINTS OCCURS IN THE MIDDLE THIRD OF THE WEAKENED PLANE (1500 - 3000 mm.), KEYED JOINT WITH THE BARS IS USED,

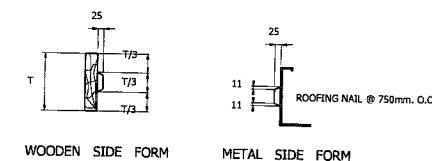
DRILLING OF HOLES ON EXISTING PCCP AND OTHER STRUCTURES WERE PERFORMED USING POWER TOOLS THE HOLES WERE PROPERLY CLEANED BEFORE GROUT/EPOXY INJECTION AND INSTALLATION OF DOWELS/TIE BARS,

THE SIDE OF EXISTING CONCRETE PAVEMENT WERE CHIPPED OFF THE ROUGHEN THE SURFACE DOWEL BARS, IT WAS PROVIDED TO CONNECT THE NEW PAVEMENT WITH THE EXISTING PAVEMENT UP TO IN DISTANCE WHERE THE FLARE IS LESS 1000mm.

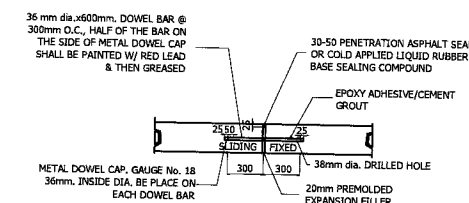
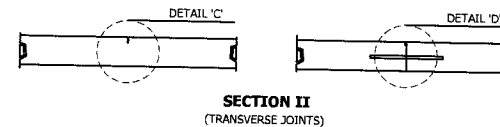
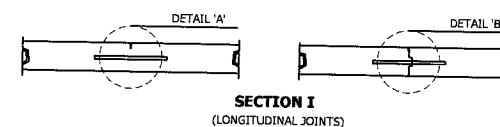
DRILLING ON HOLES ON EXISTING PCCP WERE PERFORMED USING POWER TOOLS THE HOLES WERE THOROUGHLY CLEANED BEFORE GROUT/EPOXY INJECTION AND INSTALLATION OF DOWEL BARS.

GENERAL NOTES:

1. MATERIALS AND WORKMANSHIP WERE CONFORMED WITH THE DPWH STANDARD SPECIFICATIONS FOR HIGHWAYS, BRIDGES AND AIRPORTS 2013 AND SPECIALS PROVISIONS.
2. CONSTRUCTION (CONTACT) JOINTS ARE FORMED WHEN CONCRETE ON ONE SIDE OF THE JOINT IS POURED AHEAD AND ALLOWED TO SET BEFORE POURING ON THE OTHER SIDE.
3. AT CONSTRUCTION JOINTS (LONGITUDINAL OR TRANSVERSE) WERE NO CONCRETE FROM THE LAST SLAB PLACED OVERHANGS ANY PORTION OF THE FIRST SLAB.
4. THE BARS ARE DEFORMED STEEL BARS. ALL THE DOWEL BARS BE SMOOTH ROUND STEEL BARS FREE FROM THE RUST AND OTHER THE DEFECTS WHICH MIGHT RESTRICT THEIR MOVEMENT.
5. TYPED OF WEAKENED PLAIN JOINT TO ARE USED SPECIFIED IN THE PLANS AND ONLY ONE TYPE IS USED FOR THE WHOLE PROJECT.
6. MATERIAL FOR THE METAL SIDE ARE BRAND NEW SHEET METAL GAUGE No. 18 OF BLOCK IRON FREE FROM THE RUST AND KINKS.
7. SIX (6) SUCCESSIVE DOWELLED BUTT JOINTS AT NORMAL JOINT ARE PROVIDED BEFORE ON AFTER IN EXPANSION JOINT.
8. THE GROOVE OR CRACK ABOVE JOINTS (LONGITUDINAL OR TRANSVERSE) ARE SEALED WITH 30 - 50 PENETRATION ASPHALT SEAL OR COLD APPLIED LIQUID RUBBER COMPOUND AFTER THE CONCRETE IS CURED AND BEFORE OPENING THE PAVEMENT TO TRAFFIC. ASPHALT SEAL WERE POURED IN SUCH MANNER THAT SPILLING IS PREVENTED/ELIMINATED. THIS PROVIDE A SMOOTH RIDING SURFACE.
9. ALL TRANSVERSE JOINTS, EXCEPT CONSTRUCTION JOINT CONTINUOUS FROM THE EDGE TO EDGE
10. ALL LONGITUDINAL JOINTS ARE AT INTERSECTIONS WITH NO GAPS OR OFFSETS
11. ALL DIMENSIONS ARE IN MILLIMETERS, UNLESS OTHERWISE SPECIFIED.
12. AVOID STOPPAGE OF FORMWORKS ALONG CURVES.



DETAIL OF SIDE FORM
SCALE: 1:20 M.



DETAILED OF DOWELLED EXPANSION JOINT AT
PCCP AND EXISTING STRUCTURES



REPUBLIC OF THE PHILIPPINES
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REGION VIII
TACLOBAN CITY DISTRICT
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NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY

PROJECT NAME AND LOCATION:

REPAIR/ MAINTENANCE OF DAMAGED PAVED
ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO-
STA.FE- SAN MIGUEL ROAD (S00033LT)

SHEET CONTENTS:

TYPICAL PLAN OF A FOUR LANE PAVEMENT

DRAFTED:

JONAS F. POSTRERO
ENGINEER I

PREPARED:

ELIZAR JEROME V. CLEA-AN
ENGINEER II

REVIEWED:

REY R. JEDINO
ENGINEER II

DATE:

SUBMITTED:

MARY GRACE L. GADDI
ENGINEER II, MAINTENANCE SECTION

DATE:

RECOMMENDED:

CHARLIE B. JAMPIL
OIC - ASSISTANT DISTRICT ENGINEER

DATE:

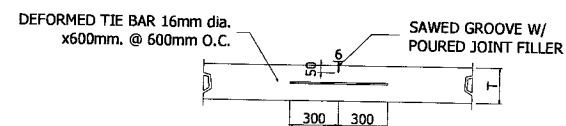
APPROVED:

RAYMUNDO
OIC - DISTRICT ENGINEER
DATE: 07/12/2025

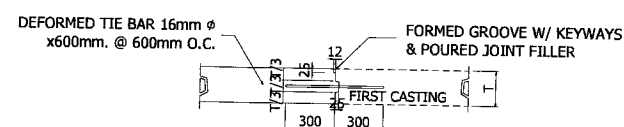
SET NO.

7
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SHEET NO.



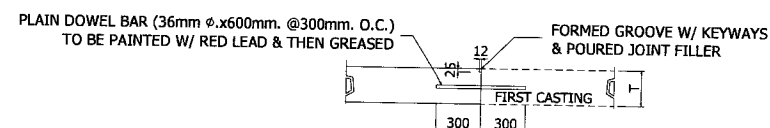
DETAIL 'A' NTS



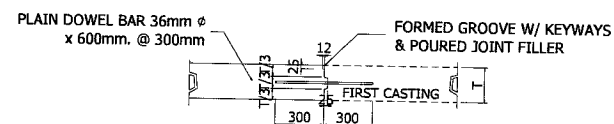
DETAIL 'B' NTS



DETAIL 'C' NTS

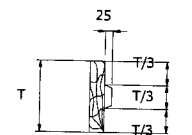


DETAIL 'D' NTS

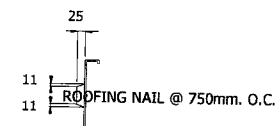


DETAIL 'E' NTS

KEYED TRANSVERSE CONSTRUCTION OR CONTRACT JOINT
(TO BE PLACED ONLY IN MIDDLE THIRD OF NORMAL JOINT INTERVAL)

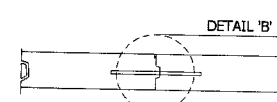
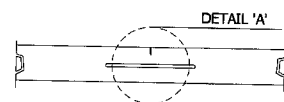


WOODEN SIDE FORM



METAL SIDE FORM

DETAIL OF SIDE FORM
SCALE: NTS



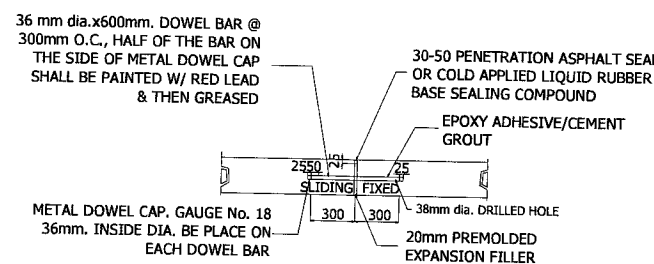
SECTION I

(LONGITUDINAL JOINTS)



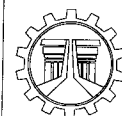
SECTION II

(TRANSVERSE JOINTS)



DETAILED OF DOWELLED EXPANSION JOINT AT
PCCP AND EXISTING STRUCTURES

AS PER D.O. 40, S. 2014			SUBSTITUTE	
THICKNESS OF PCCP (MM)	SIZE OF DOWEL BAR (MM)	SPACING (MM)	SIZE OF DOWEL BAR (MM)	SPACING (MM)
280	36	300	32	250
			28	190
			25	150
290	36	295	32	230
			28	170
			25	140
300	36	274	32	210
			28	150
			25	130
310	36	255	32	200
			28	150
			25	120
320	36	238	32	190
			28	140
			25	110
330	36	223	32	180
			28	130
			25	110
340	36	209	32	170
			28	130
			25	100



REPUBLIC OF THE PHILIPPINES
DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS
REGION VIII
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NEW BUS TERMINAL, BRGY. 81, ABUCAY, TACLOBAN CITY

PROJECT NAME AND LOCATION:
REPAIR/ MAINTENANCE OF DAMAGED PAVED
ROAD - K0907+209 - K0907+321.5 ALONG
TIGBAO- STA.FE- SAN MIGUEL ROAD (S00033L.T)

SHEET CONTENTS:
KEYED TRANSVERSE CONSTRUCTION OR
CONTRACT JOINT
DETAILED OF DOWELLED EXPANSION JOINT AT
PCCP AND EXISTING STRUCTURES

DRAFTED:
JONAS F. POSTRERO
ENGINEER
PREPARED:
ELIZAR JEROME T. BACLEA-AN
ENGINEER II

REVIEWED:
REY B. JECINO
ENGINEER II
DATE:

SUBMITTED:
MARY GRACE L. GADDI
ENGINEER II, MAINTENANCE SECTION
DATE:

RECOMMENDED:
CHARLIE B. TAMPIL
OIC - ASSISTANT DISTRICT ENGINEER
DATE:

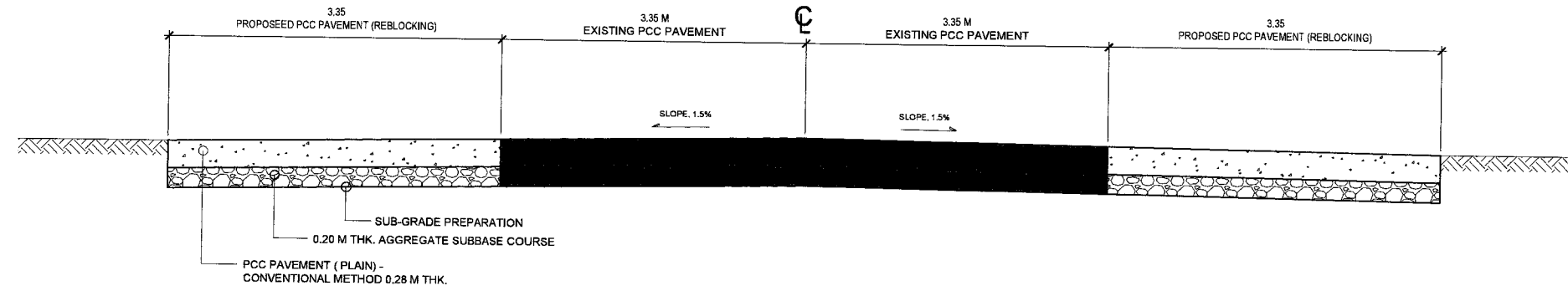
APPROVED:
RAY B. JECINO
OIC - DISTRICT ENGINEER
DATE: 07/18/2025

SET NO.

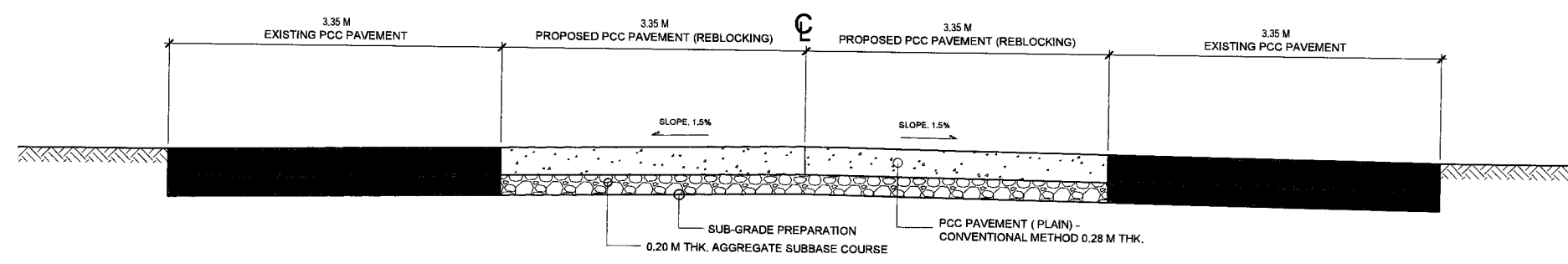
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8

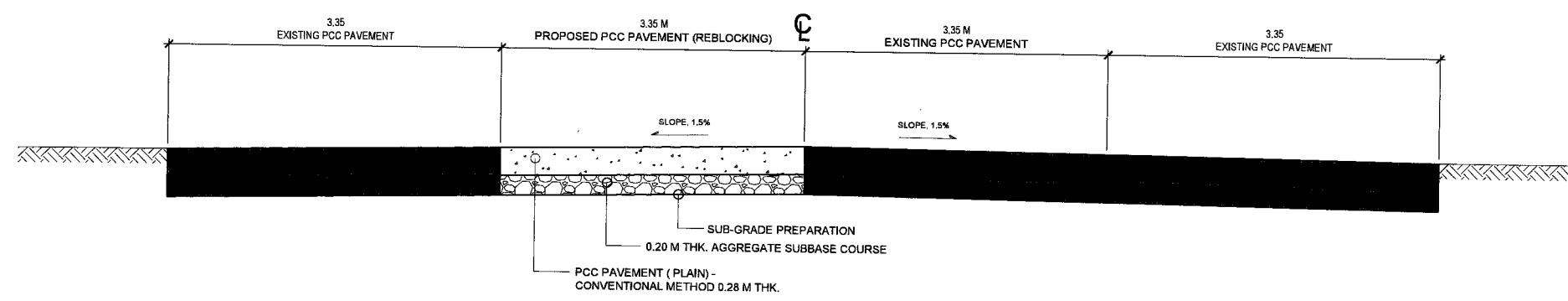
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ROADWAY SECTION 'A'
SCALE 1:100

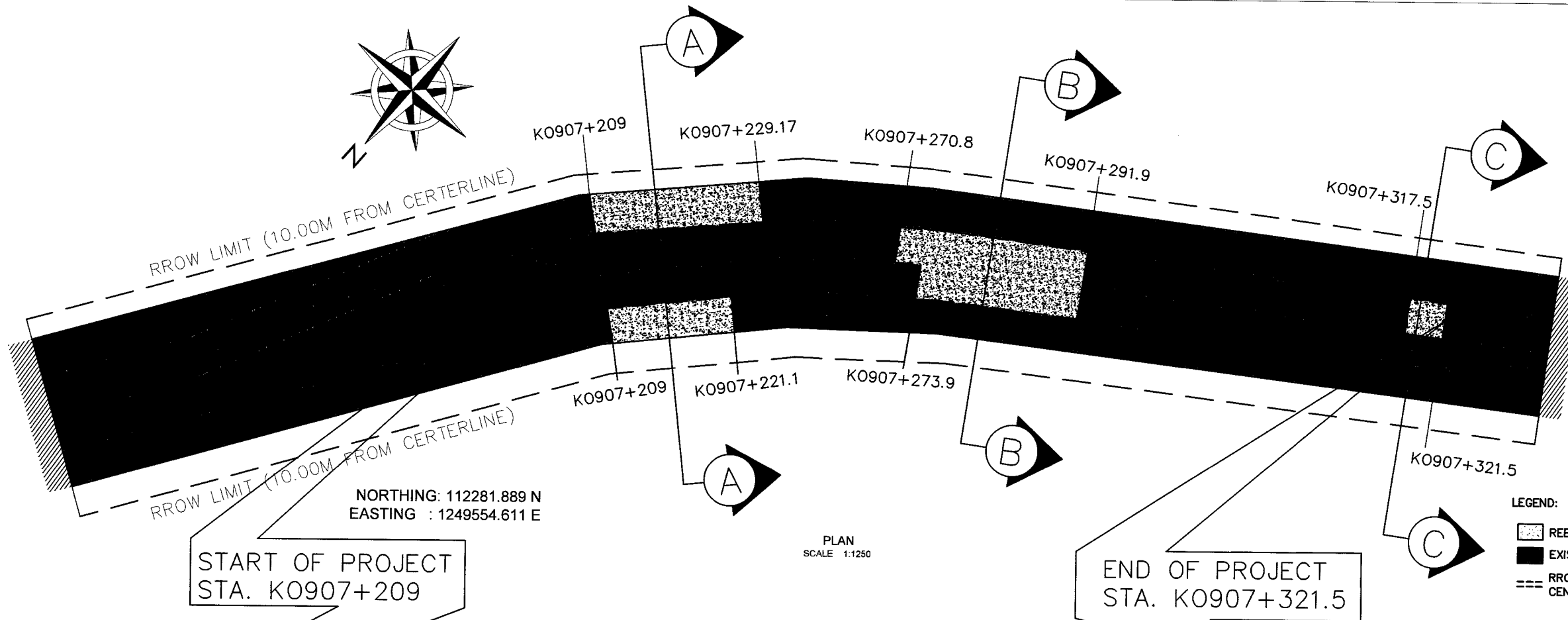


ROADWAY SECTION 'B'
SCALE 1:100



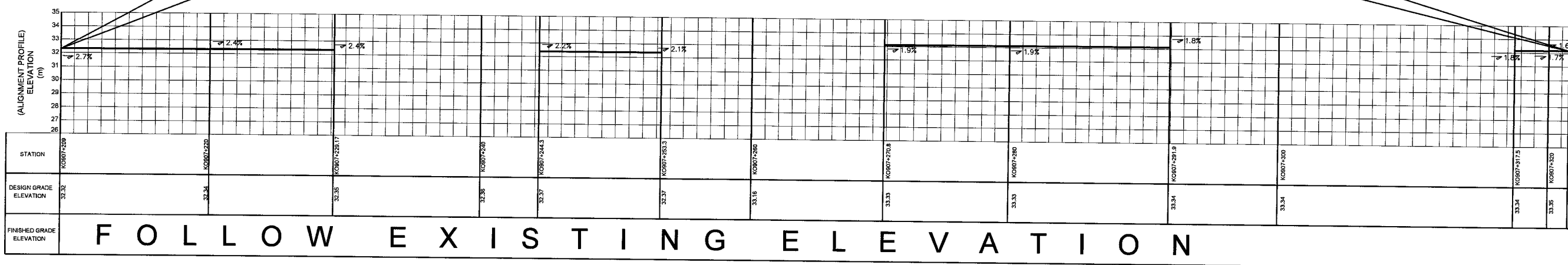
ROADWAY SECTION 'C'
SCALE 1:100

<p>REPUBLIC OF THE PHILIPPINES DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS REGION VIII TACLOBAN CITY DISTRICT ENGINEERING OFFICE NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY</p>	PROJECT NAME AND LOCATION:	SHEET CONTENTS:	DRAFTED:	REVIEWED:	SUBMITTED:	RECOMMENDED:	APPROVED:	SET NO.	SHEET NO.
	REPAIR/ MAINTENANCE OF DAMAGED PAVED ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO- STA.FE- SAN MIGUEL ROAD (S00033LT)	ROADWAY SECTION 'A' ROADWAY SECTION 'B' ROADWAY SECTION 'C'	JONAS P. POSTERO ENGINEER II	REY R. REZINO ENGINEER II	MARY GRACE C. GADDI ENGINEER II, MAINTENANCE SECTION	CHARLIE B. TAMPIL OIC - ASSISTANT DISTRICT ENGINEER	RAY R. REZINO OIC - DISTRICT ENGINEER	9 17	9 17



LEGEND:

- REBLOCKING (280MM THK. PCCP)
- EXISTING PCCP
- ROW LIMIT (10.00M FROM CENTERLINE)



PROFILE
SCALE H = 1:1250
V = 1:125



REPUBLIC OF THE PHILIPPINES
DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS
REGION VIII
TACLOBAN CITY DISTRICT
ENGINEERING OFFICE
NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY

PROJECT NAME AND LOCATION:
REPAIR/ MAINTENANCE OF DAMAGED PAVED
ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO-
STA.FE- SAN MIGUEL ROAD (S00033LT)

SHEET CONTENTS:
PLAN AND PROFILE

DRAFTED:
JONAS POSTINERO
ENGINEER I
PREPARED:
ELIZAR JEROME MACLEA-AN
ENGINEER II

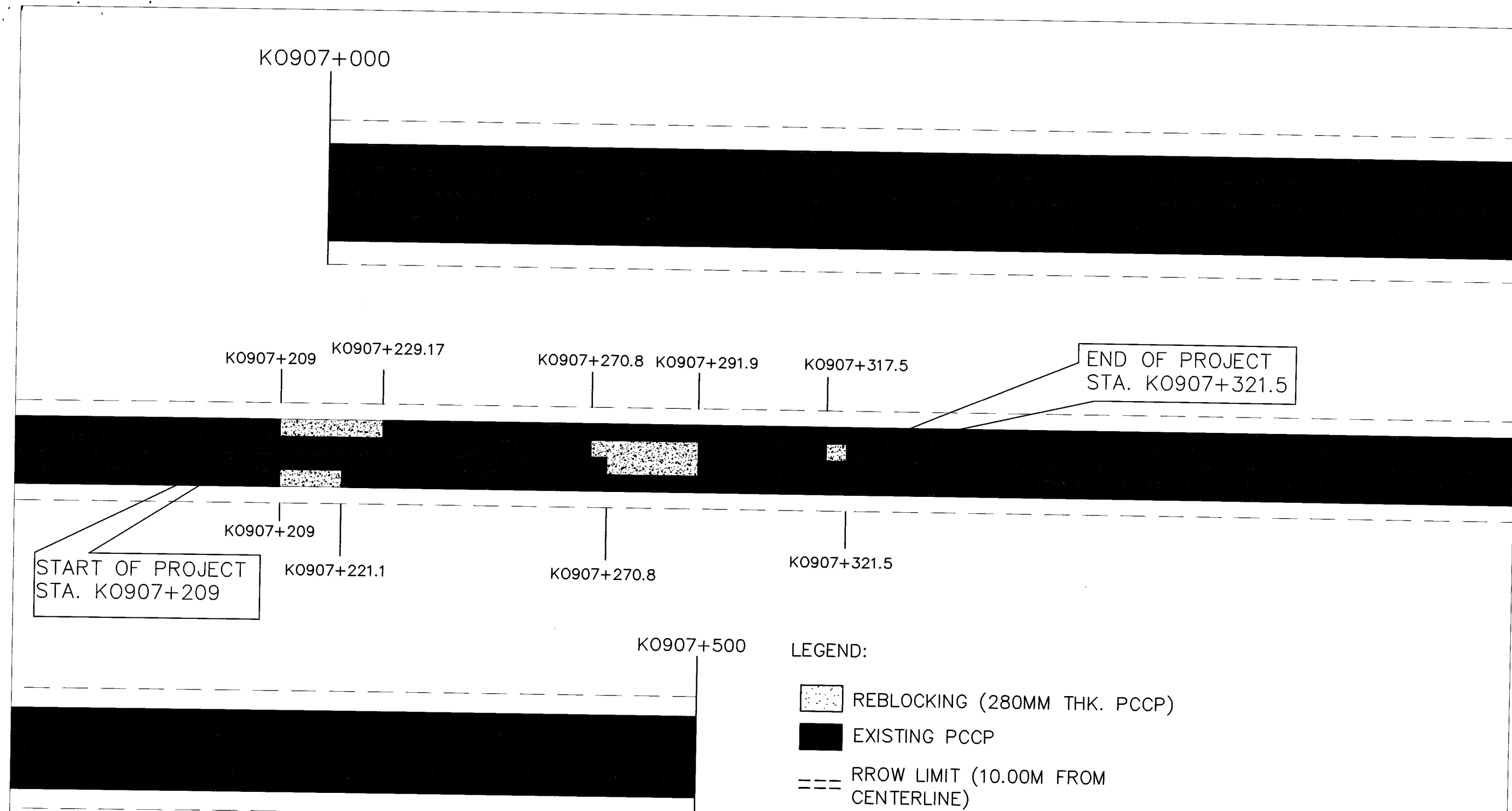
REVIEWED:
REY R. ECINO
ENGINEER II
DATE:

SUBMITTED:
MARY GRACE L. GADDI
ENGINEER II, MAINTENANCE SECTION
DATE:




RECOMMENDED:
CHARLES B. TAMPIL
OIC - ASSISTANT DISTRICT ENGINEER
DATE:

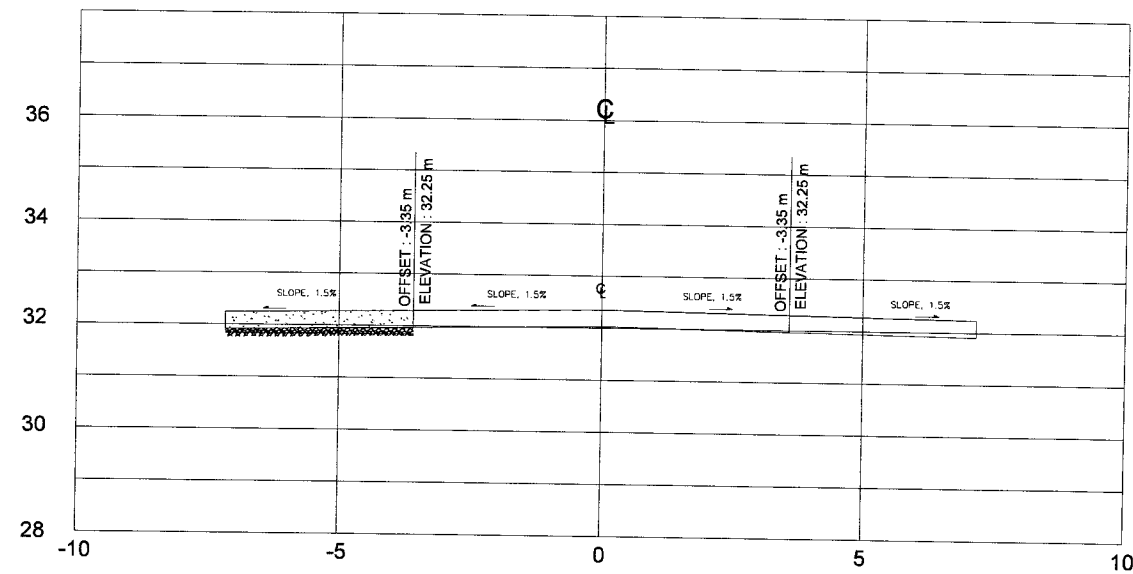
APPROVED:
RAY F. DATE
OIC - DISTRICT ENGINEER
DATE:

SET NO. SHEET NO.
10
17



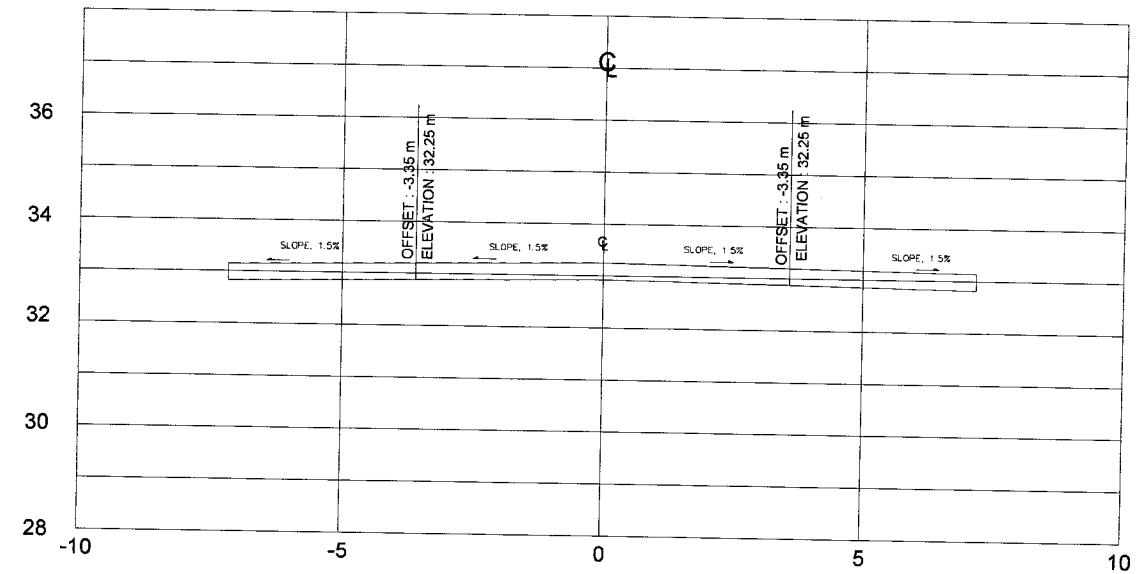
STRAIGHT LINE DIAGRAM

 <p>REPUBLIC OF THE PHILIPPINES DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS REGION VIII TACLOBAN CITY DISTRICT ENGINEERING OFFICE NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY</p>	PROJECT NAME AND LOCATION:	SHEET CONTENTS:	DRAFTED:	REVIEWED:	SUBMITTED:	RECOMMENDED:	APPROVED:	SET NO.	SHEET NO.
	REPAIR/ MAINTENANCE OF DAMAGED PAVED ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO-STA.FE- SAN MIGUEL ROAD (S00033LT)	STRAIGHT LINE DIAGRAM	<p>JONAS F. POSTRERO ENGINEER I</p> <p>ELIZAR JEROME T. BACLEA-AN ENGINEER II</p>	<p>REY R. ECINO ENGINEER II</p>	<p>MARY GRACE L. GADDI ENGINEER II, MAINTENANCE SECTION</p>	<p>CHARLIE B. TAMPIL OIC - ASSISTANT DISTRICT ENGINEER</p>	<p>RAY M. DATE OIC - DISTRICT ENGINEER</p>		



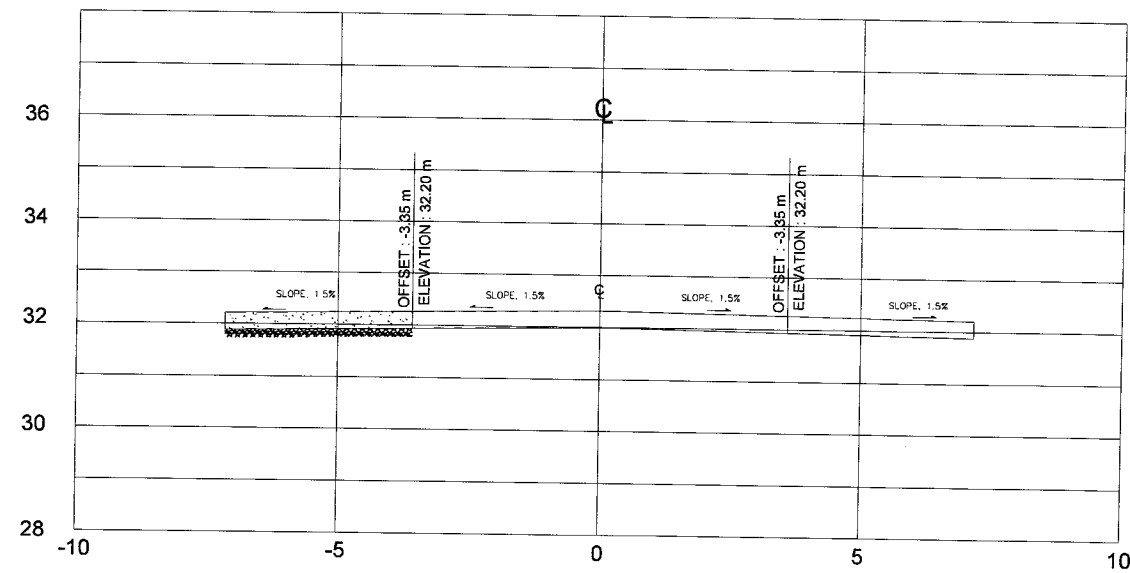
K0907+220

D.G.E = 32.34



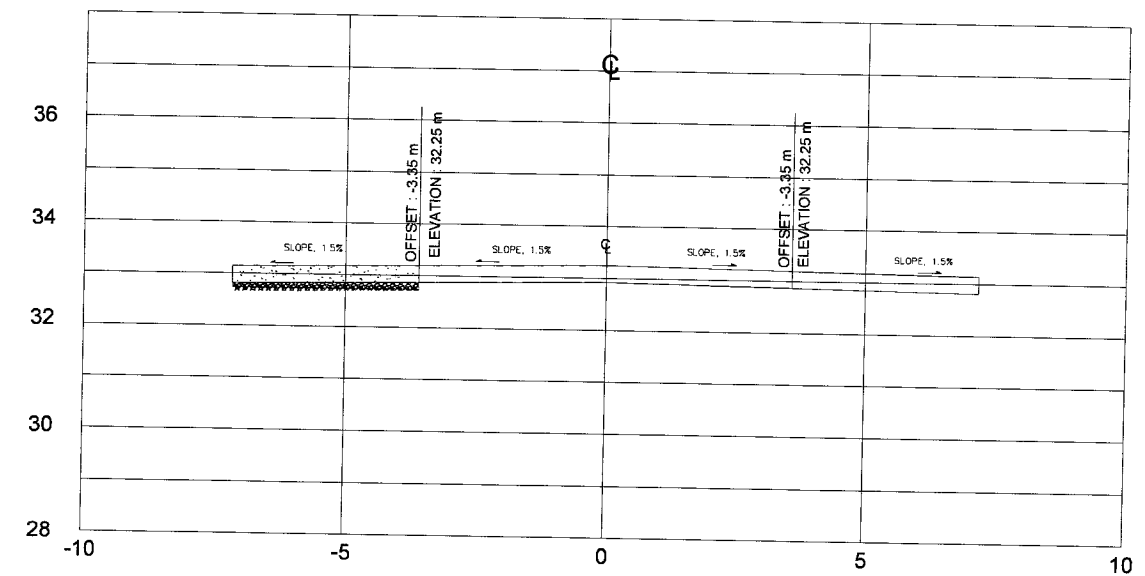
K0907+240

D.G.E = 32.36



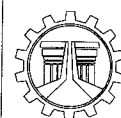
K0907+209

D.G.E = 32.32



K0907+229.17

D.G.E = 32.35



REPUBLIC OF THE PHILIPPINES
DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS
REGION VIII
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PROJECT NAME AND LOCATION:
REPAIR/ MAINTENANCE OF DAMAGED PAVED
ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO-
STA.FE- SAN MIGUEL ROAD (S00033LT)

SHEET CONTENTS:

CROSS SECTION A

DRAFTED:

JONAS F. POSTRERO
ENGINEER

PREPARED:

ELIZAR JEROME MACLEA-AN
ENGINEER II

REVIEWED:

REY R. ESCINO
ENGINEER II

DATE:

SUBMITTED:

MARY GRACE GADDI
ENGINEER II, MAINTENANCE SECTION

DATE:

RECOMMENDED:

CHARLIE TAMPIL
OIC - ASSISTANT DISTRICT ENGINEER

DATE:

APPROVED:

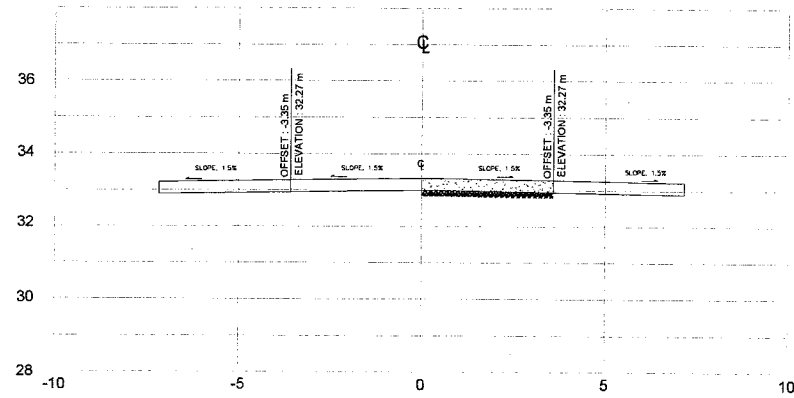
RAY F. DATE
OIC - DISTRICT ENGINEER

DATE:

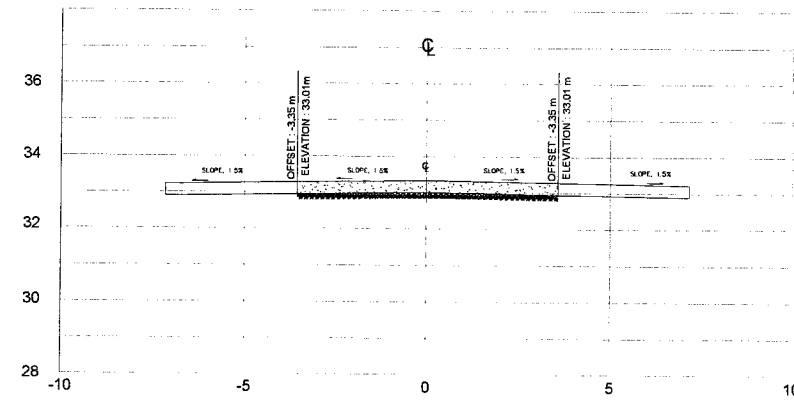
SET NO.

12
17

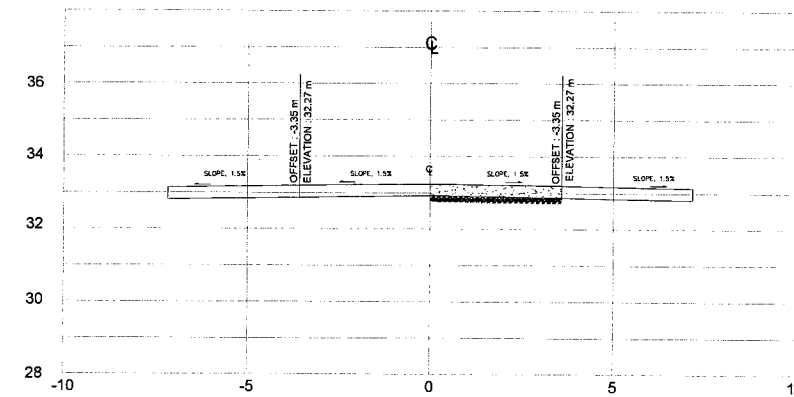
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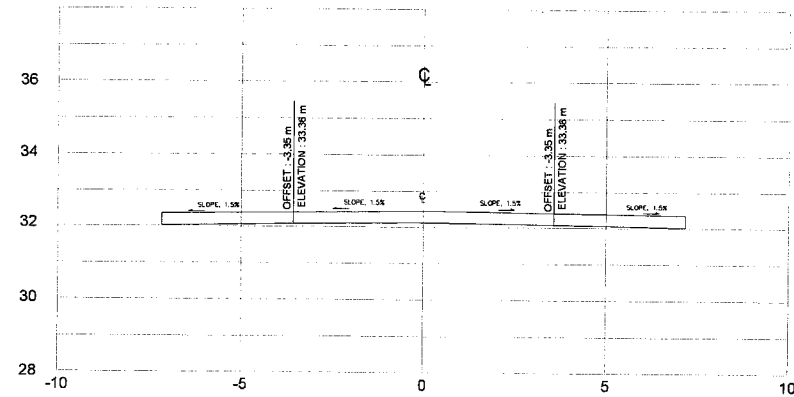
K0907+253.3
D.G.E = 32.37



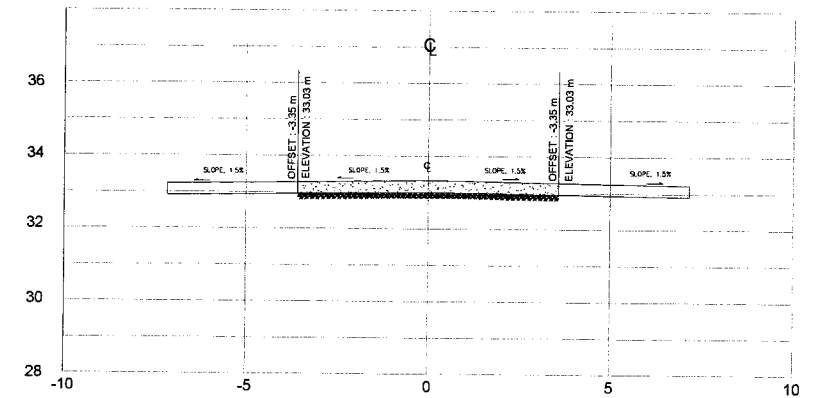
K0907+270.8
D.G.E = 33.11



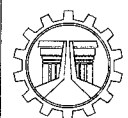
K0907+244.3
D.G.E = 32.37



K0907+260
D.G.E = 32.46



K0907+280
D.G.E = 33.13



REPUBLIC OF THE PHILIPPINES
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REGION VIII
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PROJECT NAME AND LOCATION:
REPAIR/ MAINTENANCE OF DAMAGED PAVED
ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO-
STA.FE- SAN MIGUEL ROAD (S00033LT)

SHEET CONTENTS:
CROSS SECTION B

DRAFTED:
JONAS F. POSTRERO
ENGINEER I
PREPARED:
ELIZAR JEROME M. MACLEA-AN
ENGINEER II

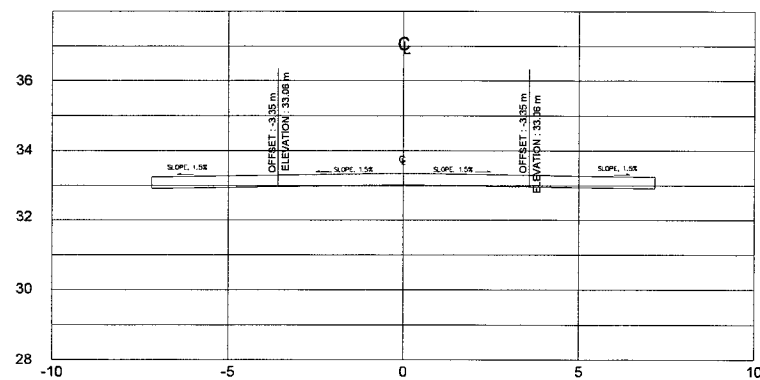
REVIEWED:
REY R. PECINO
ENGINEER II
DATE:

SUBMITTED:
MARY GRACE L. GADDI
ENGINEER II, MAINTENANCE SECTION
DATE:

RECOMMENDED:
CHARLIE B. TAMPIL
OIC - ASSISTANT DISTRICT ENGINEER
DATE:

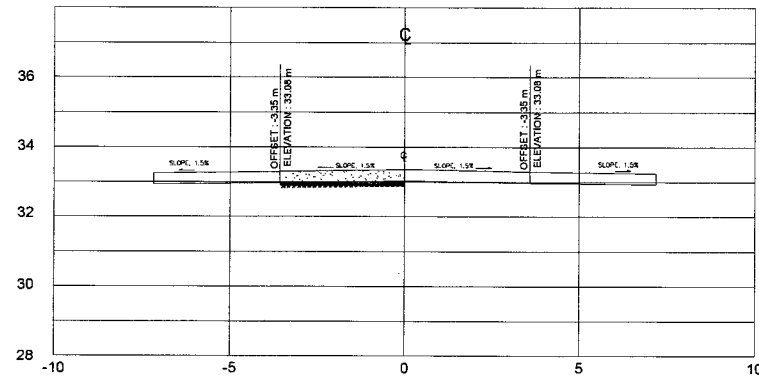
APPROVED:
RAY M. DATE
OIC - DISTRICT ENGINEER
DATE: 01/18/2016

SET NO. SHEET NO.
13
17



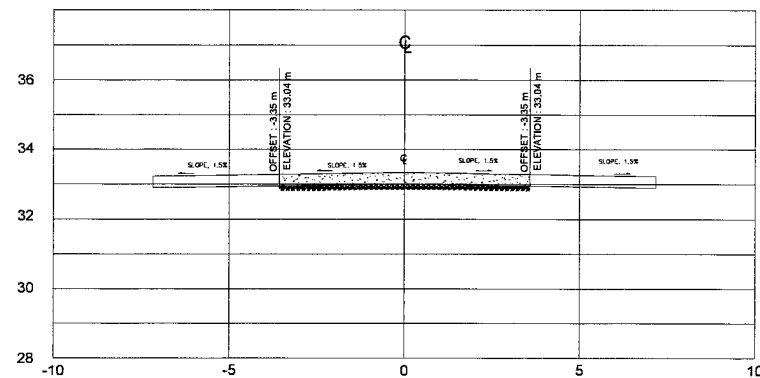
K0907+300

D.G.E = 33.16



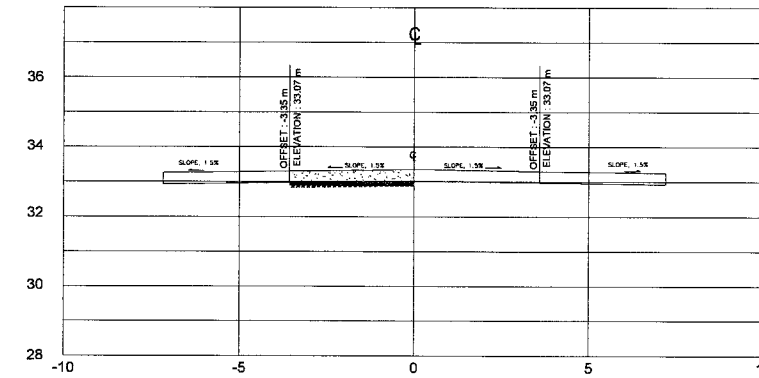
K0907+320

D.G.E = 33.18



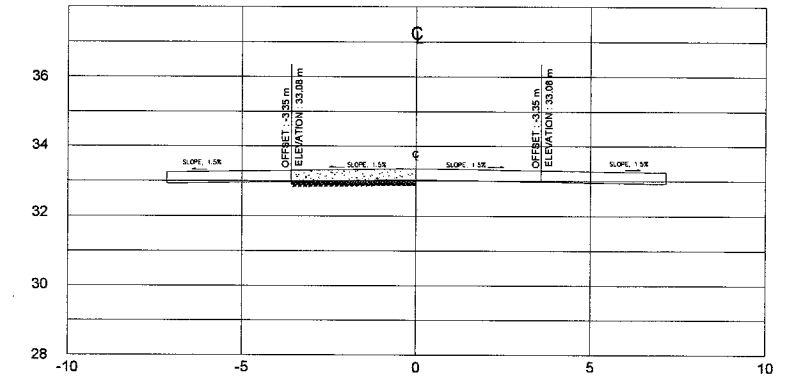
K0907+291.9

D.G.E = 33.14



K0907+317.5

D.G.E = 33.17



K0907+321.5

D.G.E = 33.18



REPUBLIC OF THE PHILIPPINES
DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS
REGION VIII
TACLOBAN CITY DISTRICT
ENGINEERING OFFICE
NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY

PROJECT NAME AND LOCATION:
REPAIR/ MAINTENANCE OF DAMAGED PAVED
ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO-
STA.FE- SAN MIGUEL ROAD (S00033LT)

SHEET CONTENTS:

CROSS SECTION C

DRAFTED:
JONAS F. POSTRERO
ENGINEER
PREPARED:
ELIZAR JEROME BACLEA-AN
ENGINEER

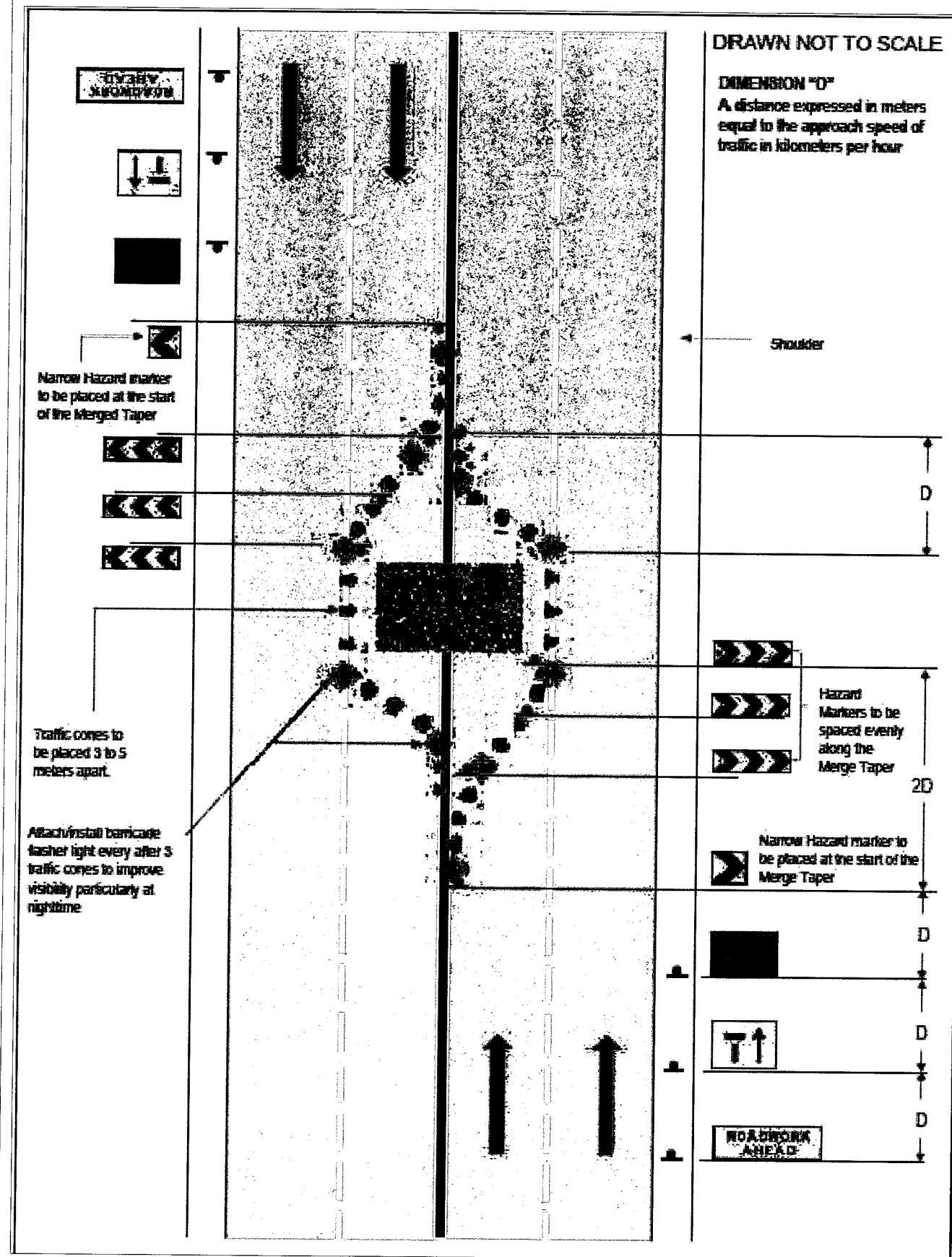
REVIEWED:
REY R. REINO
ENGINEER II
DATE:










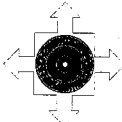
SUBMITTED:
MARY GRACE C. GADDI
ENGINEER II, MAINTENANCE SECTION
DATE:

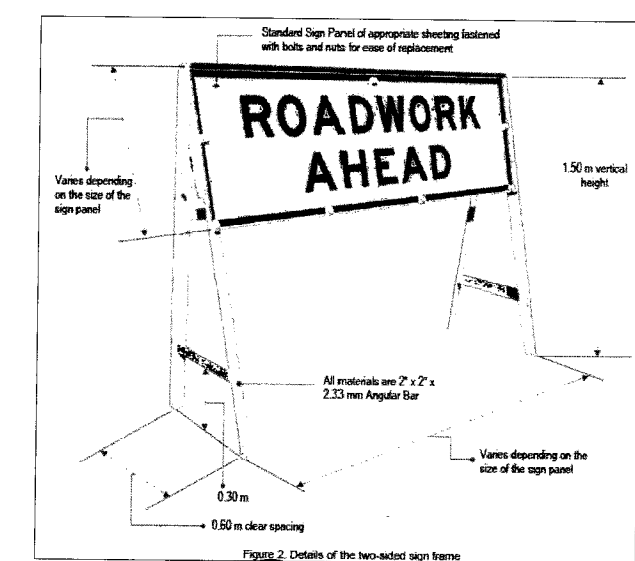
RECOMMENDED:
CHARLIE S. TAMPIL
OIC - ASSISTANT DISTRICT ENGINEER
DATE:


APPROVED:
RAY M. DATE
OIC - DISTRICT ENGINEER
DATE: 07/18/2025

SET NO. SHEET NO.
14
17



SCHEDULE OF TRAFFIC MANAGEMENT SIGNS					
		Sign No.	Size (mm) (Width X Height)	Letters/Symbols	Background
	ROADWORK AHEAD (T1-1, T1-31)	T1-1	1800 X 600	Line 1 - White 200 DM Line 2 - White 160 DM	Yellow ReflectORIZED
	WORKMEN AHEAD (Symbolic) (T1-5)	T1-5	900 X 600	Black	Red / Orange -Fluorescent for day use (Short Term) -ReflectORIZED for night use (Long Term)
	END ROADWORK (T2-16, T2-17)	T2-16	1800 X 600	Line 1 - White 200 DM Line 2 - White 160 DM	Yellow ReflectORIZED
	PREPARE TO STOP (T1-18)	T1-18	900 X 600	Line 1 - White 120 DM Line 2 - White 120 DM Line 3 - White 120 EM ReflectORIZED	Red ReflectORIZED
	TEMPORARY HAZARD MARKER (T5-5)	T5-5	600 X 600 Type B-3	Chevrons Black 194 wide at 45°	Yellow ReflectORIZED
	ROAD MACHINERY AHEAD (T1-3)	T1-3	900 X 600	Line 1 - Black 100 EM Line 2 - Black 120 DM Line 3 - Black 100 EM	Yellow ReflectORIZED
	PEDESTRIANS (T8-2 L or R)	T8-2 L or R	1200 X 300	BLACK 100 CM ARROW 140	Yellow ReflectORIZED
					
				Traffic Cones	
				Traffic Bollards with Barricade Flasher Light	



 <p>REPUBLIC OF THE PHILIPPINES DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS REGION VIII TACLOBAN CITY DISTRICT ENGINEERING OFFICE NEW BUS TERMINAL, BRGY. 91, ABUCAY, TAGLOBAN CITY</p>	PROJECT NAME AND LOCATION: REPAIR/ MAINTENANCE OF DAMAGED PAVED ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO- STA.FE- SAN MIGUEL ROAD (S00033LT)	SHEET CONTENTS: SCHEDULE OF TRAFFIC MANAGEMENT SIGNS DETAILS OF TRAFFIC MANAGEMENT SIGNS	DRAFTED: JONAS F. POSTERO ENGINEER II	REVIEWED: REY R. JECINO ENGINEER II	SUBMITTED: MARY GRACE L. GADDI ENGINEER II, MAINTENANCE SECTION	RECOMMENDED: CHARME B. TAMPIL OIC - ASSISTANT DISTRICT ENGINEER	APPROVED: RAY R. JECINO OIC - DISTRICT ENGINEER	SET NO. 15	SHEET NO. 17
	<p>DATE: _____</p>								



Government Center Candahug, Palo,
Leyte

Project : _____

Cost : _____

Location : _____

Fund Source/s : _____

Implementing Agency/ies : _____

Development Partner/s : _____

Contractor/Supplier : _____

Brief Description of Project : _____

Project Details :

Project Date			Project Status				
Duration	Started	Target Date of Completion	Percentage of Completion	As of (Date)	Cost Incurred to Date	Date Completed	Remarks

For particulars or complaints about this project, please contact the Regional Office or Cluster which has audit jurisdiction on this project :

COA Regional Office No./Cluster : _____

Address : _____

Contact No. : _____ or Text COA Citizen's Desk at 0915-5391957

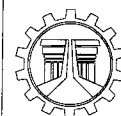
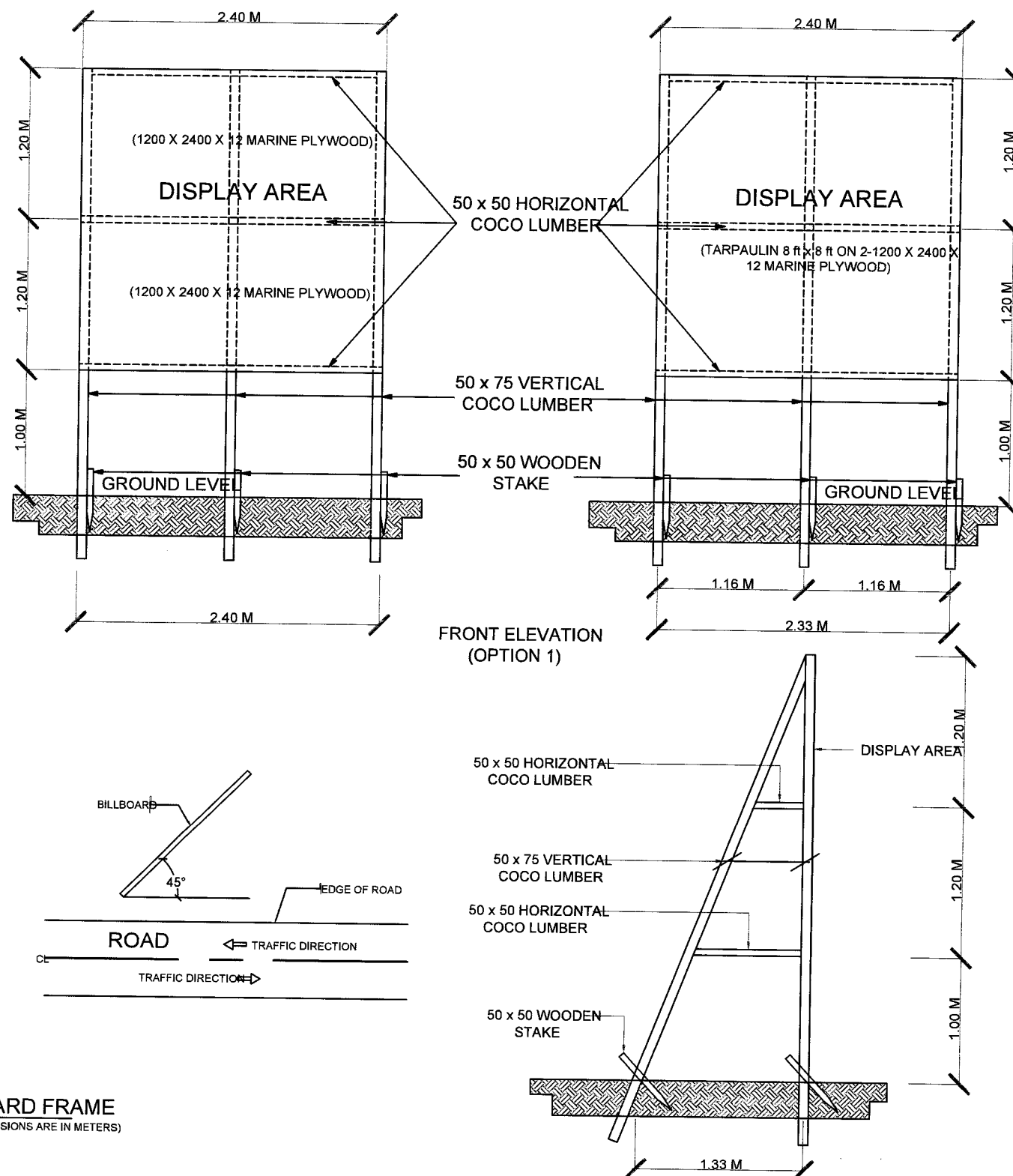
COA STANDARD PROJECT BILLBOARD

SPECIFICATIONS:

1. TARPAULIN, WHITE, 8 FT X 8 FT
2. RESOLUTION : 70 DPI
3. FONT : HELVETICA
4. FONT SIZE : MAIN INFORMATION - 3"
SUB-Information - 1"
5. FONT COLOR: BLACK
6. BACKGROUND COLOR: WHITE

COA BILLBOARD FRAME

(NOT TO SCALE, ALL DIMENSIONS ARE IN METERS)



REPUBLIC OF THE PHILIPPINES
DEPARTMENT OF PUBLIC WORKS AND HIGHWAYS
REGIONAL OFFICE NO. VIII
TACLOBAN CITY DISTRICT
ENGINEERING OFFICE
NEW BUS TERMINAL, BRGY. 91, ABUCAY, TACLOBAN CITY

PROJECT NAME AND LOCATION:

REPAIR/ MAINTENANCE OF DAMAGED PAVED
ROAD - K0907+209 - K0907+321.5 ALONG TIGBAO-
STA.FE- SAN MIGUEL ROAD (S00033LT)

SHEET CONTENTS:

COA BILLBOARD

DRAFTED:

JONAS T. POSTERO
ENGINEER I

PREPARED:
ELIZAR JEROME T. RACLEA-AN
ENGINEER

REVIEWED:

REY R. RECINO
ENGINEER II
DATE: _____

SUBMITTED:

MARY GRACE L. GADDI
ENGINEER II, MAINTENANCE SECTION
DATE: _____

RECOMMENDED:

CHARLIE B. TAMPIL
OIC - ASSISTANT DISTRICT ENGINEER
DATE: _____

APPROVED:

RAY P. DATE
OIC - DISTRICT ENGINEER
DATE: 07/18/2016

SET NO.

17

SHEET NO.

17